

MASTER DEVELOPMENT PLAN

FOR THE CUYAHOGA RIVERFRONT

February 7th 2025

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EXECUTIVE SUMMARY

Background

Bedrock Cleveland and the City of Cleveland have partnered on this once-in-a-lifetime opportunity to develop a unique and dynamic 35-acre transit-oriented neighborhood in downtown Cleveland along the banks of the Cuyahoga River. The Cuyahoga Riverfront at Tower City Center will be a new neighborhood where Cleveland residents can live, work, gather, and connect with the riverfront. The project is focused on mobility, accessibility, equity, opportunity, and sustainability. It aims to leverage the best features and strategies to place Cleveland at the forefront of innovative urban riverfront and transit-oriented development.

Bedrock's mission is to uncover, acquire, develop, and manage real estate opportunities that make Cleveland even more exciting and prosperous. In 2023, this vision for the riverfront was presented to the Cleveland Planning Commission, and in May of that year, the Cuyahoga Riverfront Master Plan received approval as the conceptual framework.

This document represents the next step in advancing Bedrock's vision for the Cuyahoga Riverfront. The purpose of this document is to further translate the vision contained in the 2023 Cuyahoga Riverfront Master Plan (CRMP) into a physical form and architectural character that reflects the surrounding context, history, and goals of the project.

Vision

The Cuyahoga Riverfront will be a neighborhood where community and optimism thrive and opportunity abound. It is where people from all over Cleveland can access and rediscover the Cuyahoga River, writing its next chapter as a recreational and economic resource.

Bedrock's vision for the Cuyahoga Riverfront is to serve as more than just a real estate investment or place for entertainment. By creating a transit-accessible neighborhood on the edges of the Cuyahoga River and downtown, the City of Cleveland can improve access to the riverfront, create new links between communities, and ensure equitable economic opportunities. This vision represents a significant investment in Cleveland's leadership, residents, organizations, and potential.

This neighborhood will feature a mix of buildings and programming with parks, open space, and the riverfront at every turn. The plentiful and accessible green spaces along the river draw inspiration from the "Forest City." A Riverfront Park and Riverwalk anchor the development, weaving together a variety of textured, cultured, and equitable public spaces that cater to diverse interests and ages.

The Cuyahoga Riverfront is also envisioned as an economic driver for downtown Cleveland, resulting in transformational, development-ready parcels sharing public, sustainable, and resilient infrastructures. Distinctive features include links from Public Square to the riverfront, emphasizing and supporting the City of Cleveland's Shore-to-Core-to-Shore initiatives. These connections unify this transit-oriented neighborhood by fostering a district where sustainability, community, recreation, and imagination can grow into new opportunities, spaces, and solutions. It is a place that aims to link the riverfront, transit, trails, commerce, culture, and community by achieving the following objectives:

- Develop downtown Cleveland as a 15-minute neighborhood that positions Cleveland towards becoming an 18-hour city.
- Leverage sustainable infrastructure and public spaces as a driver for investment.
- Ensure equitable delivery of economic growth and equitable opportunities for Clevelanders.

Purpose

The role of this document is to provide a framework for the development of buildings, parks, places, spaces, and streets. A complementary document to the Master Development Agreement (MDA) between Bedrock Cleveland and the City, this updated Master Plan, presented in the subsequent chapters contained within are designed to ensure the approved vision and supporting benefit agreements and legislative actions are executed and implemented in a high-quality, consistent, and complementary manner to the larger riverfront and urban contexts. The content is meant to provide guidance and not a proscriptive path for the development. Design creativity, not uniformity, is the goal and intent of this document.

Process

This Master Plan results from consultation and collaboration with the City of Cleveland and the Bedrock Team. It is intended to be used as a tool and in conjunction with the Cleveland Planning Commission's ongoing conceptual, schematic, and final design review process to guide and manage the projects' development over several decades. As phases are completed and conditions warrant, this document may be updated as needed to ensure it maintains design flexibility while preserving the district's vision and objectives.

Principles

The following principles represent various values and criteria to help realize the Cuyahoga Riverfront Master Plan's objectives by utilizing key urban design strategies within the built environment. Each principle represents a fundamental ethos to develop a cohesive and thriving district.

I. Mobility:

Leverage transit access and adjacency to the Central Business District to create an interconnected multimodal transportation network that shifts mode use and reduces reliance on the automobile.

- District and Regional Connectivity
- Pedestrian and Bicycle Facilities
- Seamless Mode Changes
- Parking and Service Access

II. Streetscapes:

Create an engaging and vibrant public realm that promotes day-to-day activity, establishes a distinct sense of place, and supports Cleveland's Complete and Green Streets Ordinance.

- Balance of Roadway Users and Zones
- Pedestrian-Scaled Design and Access
- Activated Sidewalks
- Public Realm and Placemaking
- Green Streets and Landscaping

III. Open Spaces:

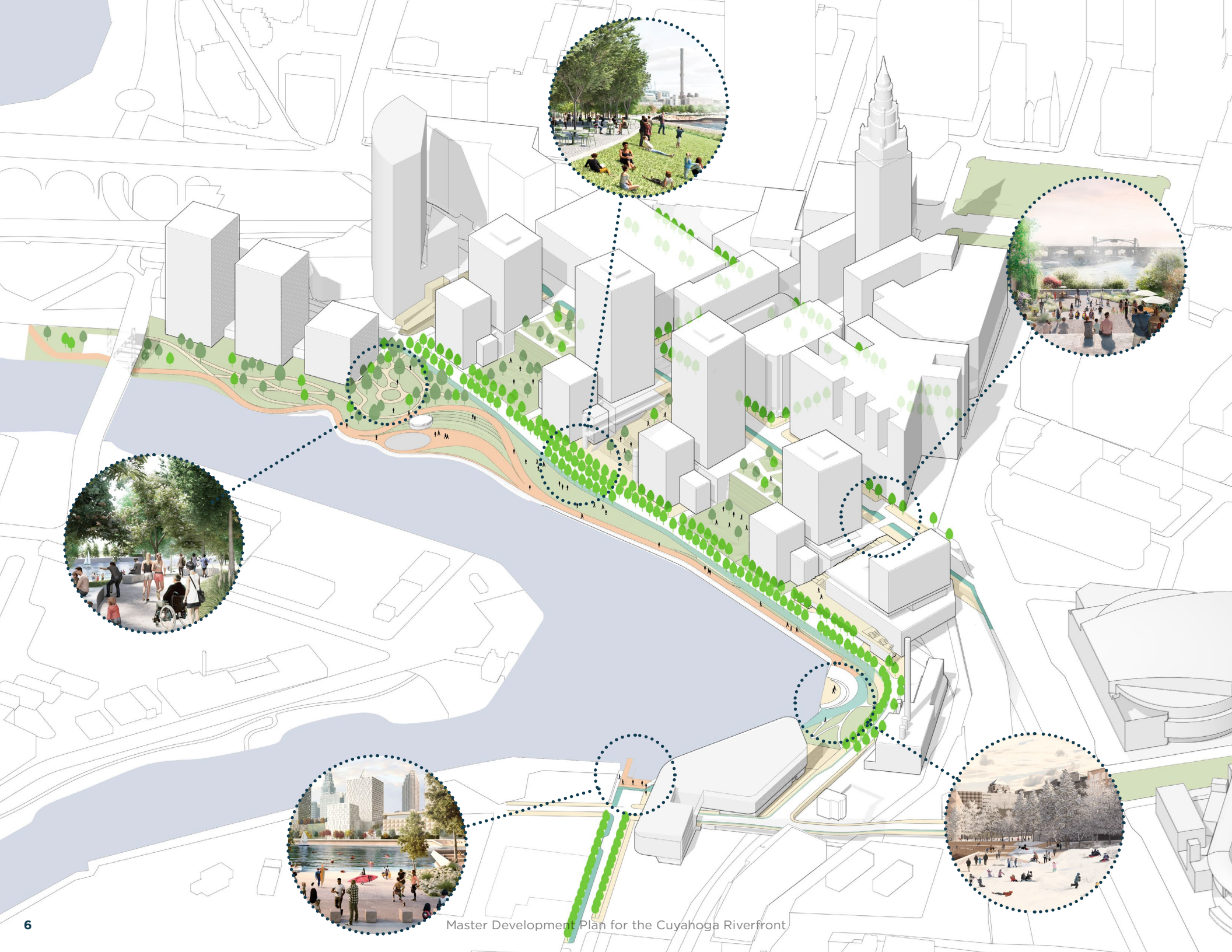
Curate a series of unique, flexible open spaces that support a meaningful sense of community, celebrate the Cuyahoga River, and meet the neighborhood's needs. Topics covered include:

- Diversity and Quality
- Interconnected
- Multi-Functional
- Equity
- Ecology

IV. Buildings:

Integrate the Cuyahoga Riverfront Master Plan with downtown Cleveland by strategically mixing land and building uses to create an enduring, aesthetically pleasing, and timeless space through solid design, appropriate building features, and lasting materials that positively contribute to the sense of place.

- Diverse Uses and Activated Ground-floors
- Form, Position, Scale, and Articulation
- Materials, Aesthetics, and Character
- Screened Parking and Service Access
- Prioritize the Public Realm



Guidelines for Public Infrastructure

The public realm and its enabling infrastructure are the foundation of the Cuyahoga Riverfront Master Plan. This chapter outlines the creative mobility solutions and riverfront features that are key outcomes of this project. By providing direction on the location, style, and quality of the parks and streets within the Cuyahoga Riverfront Project Area, this chapter ensures the character and amenities built through future public and private investments meet the goals and aspirations of the plan and the greater Cleveland community.

Public Infrastructure Objectives:

- Provide guidance and coordination on infrastructure recommendations that are broad enough to realize the Cuyahoga Riverfront Master Plan (CRMP) regardless of location or type and to ensure these recommendations align with adjacent infrastructure, both existing and proposed.
- Leverage any community investments in public transit, at tower city center to support new development and expand access to the Cuyahoga Riverfront
- Ensure investments in infrastructure and the public realm are planned, designed, and implemented in an orderly, efficient, and high-quality manner.
- Instill predictability and confidence that the district's infrastructure and public realm components such as the Riverwalk will be consistent and high-quality throughout project buildout - from the first sequence to the last.
- Allow flexibility in the design and programming of open spaces and the public realm while ensuring that each project contributes in a complementary manner to the district's infrastructure and mobility goals.

Architectural Guidelines for Private Development

The intent of this section is to provide a framework for architectural design that can accommodate creative solutions, market conditions, and changing technologies and meet the evolving needs and aspirations of the community. Consequently, it is imperative that this chapter provide direction to reflect the broader goals of the development while not demanding that projects adhere to specific architectural design styles nor mandate design standards for uniformity or conformity.

Architectural Guidelines Objectives:

- Provide design guidance broad enough to encompass all structures across the area of development, regardless of scale and/or program.
- Enable a variety of building types and styles throughout the riverfront. Buildings should be a balance of fabric (background) buildings and object (iconic) buildings. There are opportunities for an iconic design where a building will be sited within predominate viewsheds and with strong relationships to adjacent structures.
- Utilize a building material palette that indicates the potential material options but does not dictate architectural design style and remains open-ended to accommodate future advancements in technology and manufacturing that will occur over the lifetime of the master plan.
- Allow flexibility in the architectural language used on a project by project basis as the district grows and new technologies, materials, and opportunities are created and discovered.





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1.
INTRODUCTION

BACKGROUND

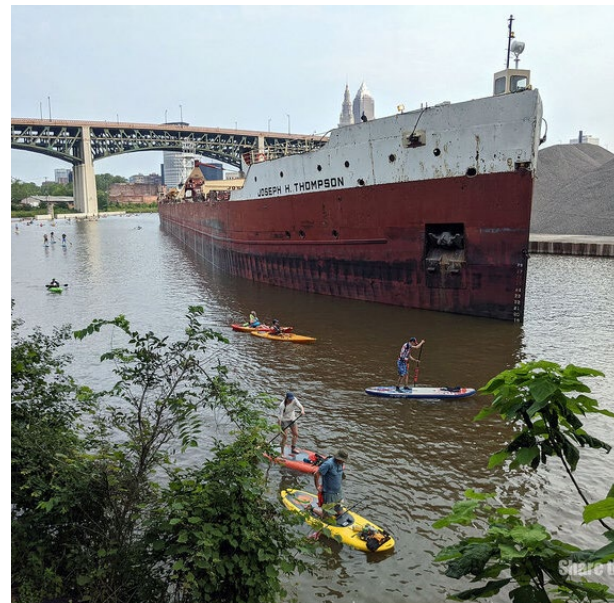
History + Momentum

A Natural and Economic Resource

The Cuyahoga River's name has various origins; it is believed to originate from the Mohawk Indian word 'cayagaga' meaning "crooked river," while others believe it may come from the Seneca word 'Cuyohaga' or "place of the jawbone." Similar to its etymology, the river itself has a deep history and has morphed through various roles, including transportation spine, industry hub, environmental battleground, and recreational rebirth. Initially an industrial artery, Cleveland's riverfront suffered neglect, becoming notorious as an "open sewer" by the late 19th century. Federal intervention widened the river mouth, first in 1826 and last in 1936, to accommodate freight traffic, but industrialization took a toll, culminating in the infamous 1969 fire.

Despite its historical significance, poor public access and pollution plagued the river's identity, epitomized by the 1969 fire that spurred national ecological action and the creation of the U.S. Environmental Protection Agency (EPA). The river's status as one of America's most polluted prompted extensive cleanup efforts, including interceptor sewer systems and federal protection under Cuyahoga Valley National Park. Despite ongoing challenges, restoration projects focus on water quality and ecosystem rehabilitation.

In the 21st century, the river embraces a dual identity, balancing its industrial heritage with recreational opportunities. While still a vital commercial route—maintaining Cleveland's prominence as a steel maker and providing efficient distribution of raw materials for manufacturing and construction—efforts to improve water quality have increased human enjoyment, with activities like paddling and rowing gaining popularity. Despite tensions between industry and recreation, advocacy groups are championing the river's role as both an economic asset and environmental treasure, as reflected in the Cuyahoga Riverfront Master Plan, which envisions a waterfront that serves all Cleveland residents equitably.



A Historic Transit Hub

Built-in 1929, Terminal Tower and its rebirth as Tower City Center remain prominent features of Cleveland's skyline and hub of transportation and business activity. Built as the centerpiece of Cleveland's Union Terminal, it has become a symbol of the city and stands as one of Cleveland's most historic and iconic locations.

The Union Terminal building complex operates as a network of interconnected structures near Public Square in downtown Cleveland. The influential local real estate developers and railroad owners Oris and Mantis Van Sweringen initially conceptualized this complex. What began as a modest terminal near Public Square soon evolved into a grand central rail station for the entire city, with dedicated tracks for efficient inner-city and inter-urban service.

To this day, the Terminal Tower and Tower City Center remain Cleveland's city within a city, as it includes the Avenue Shopping Mall, Jack Cleveland Casino, Hotel Cleveland, Chase Financial Plaza, and the GCRTA Tower City Center Rapid Station—the central hub of GCRTA's light and heavy rail transit system. These rail lines are vital in the region, serving Cleveland Hopkins International Airport to the west, the Lake Erie waterfront to the north, and several suburbs to the east, west, and south of downtown.



Synergies with Related Projects

The Cuyahoga Riverfront Master Plan leverages and proposes several synergies with existing efforts throughout Cleveland and the region; these include the North Coast Connector Project, Public Square Bollard Improvements, Cleveland's Complete and Green Street Ordinance, Cleveland's Citywide Mobility Plan, the Cuyahoga Valley Scenic Railroad Extension, and the Memorial Bridges Loop Trail. In addition to these, two critical efforts the Cuyahoga Riverfront Master Plan reinforces are the Vision for the Valley and the Shore-to-Core-to-Shore visions.

Vision for the Valley

This plan divides the river into four sections - The Old River Channel Area, Collision Bend, Jefferson Link, and Big Creek Gateway - creating a framework for land use, mobility, and public space recommendations. Collision Bend recommendations - home to the Cuyahoga Riverfront Master Plan project area - include expanding accessibility to the river via a riverfront park, terraced landscapes up to Huron Road and Ontario Street, and supporting increased activity through mixed-use infill development along Huron Road and the Cuyahoga River.

Shore-to-Core-to-Shore

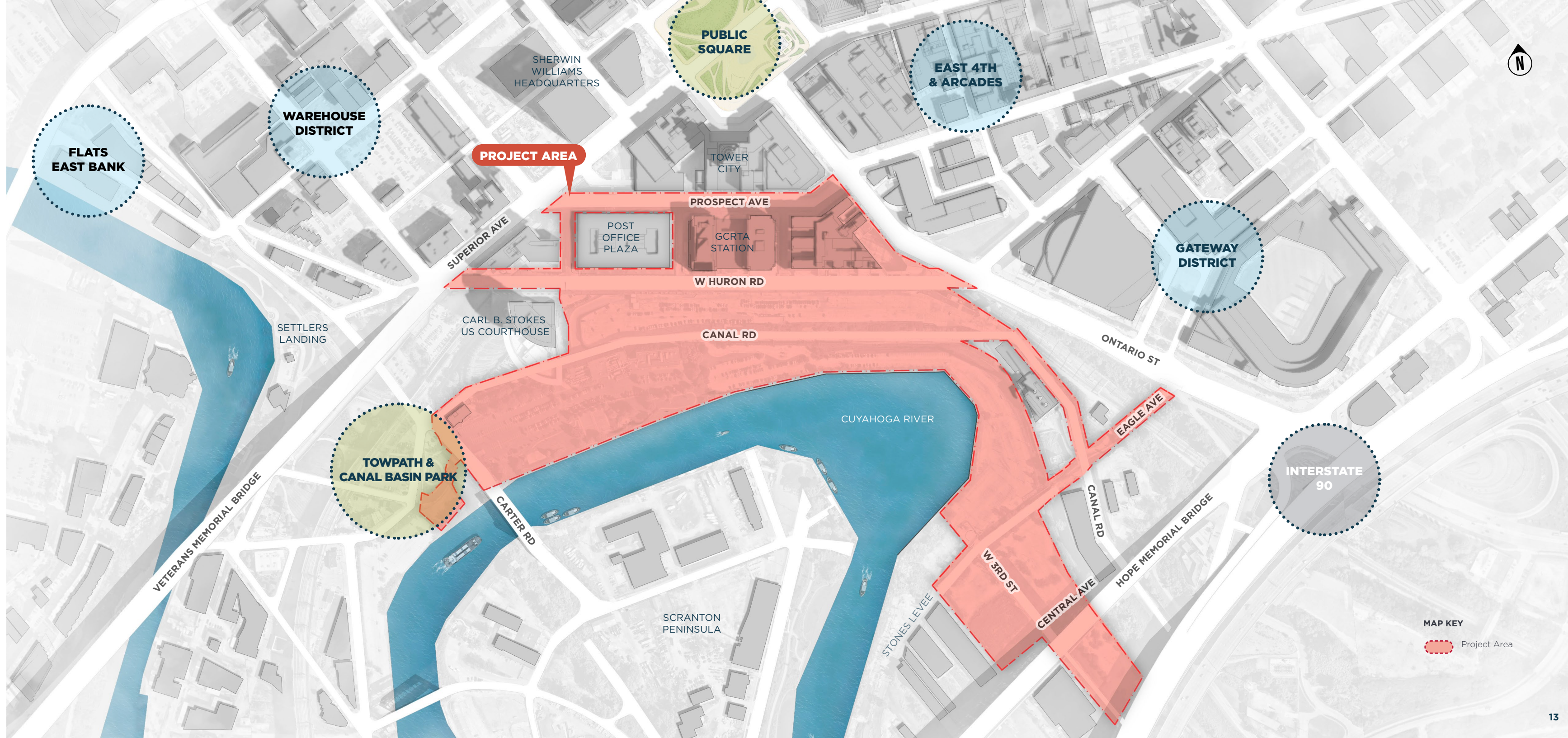
The vision of Shore-to-Core-to-Shore is to establish Cleveland as a vibrant dual waterfront city that creates dynamic waterfront neighborhoods fostering equitable access to parks along the Cuyahoga River and Lake Erie, Public Square, and the malls of downtown Cleveland. This initiative aims to improve the lakefront and riverfront and enhance connectivity between the downtown core and these new strategic investments, leveraging these natural resources and downtown Cleveland's unique position as a driver for economic growth and public and private investments. In 2024, a new tax increment financing (TIF) District was established to create, capture, and leverage growth in Cleveland's Downtown to support public improvements that will transform the city's waterfronts.

LOCATION

Project Area

Project Area

Directly adjacent to the Cuyahoga River and parts of downtown Cleveland, the Cuyahoga Riverfront Project Area is uniquely positioned to leverage this historic site to develop Ohio's largest transit-oriented neighborhood. With immediate access to Tower City Center and Public Square for GCRTA bus and rail service and the Towpath Trail for walking and biking, this site has mobility and 15-minute city principles at its core. Beyond mobility, the project site is in immediate proximity to various entertainment, jobs, trails, open spaces, and cultural amenities downtown Cleveland offers, becoming a place of residence, relaxation, employment, activation, and equitable opportunity for Cleveland and Northeast Ohio.



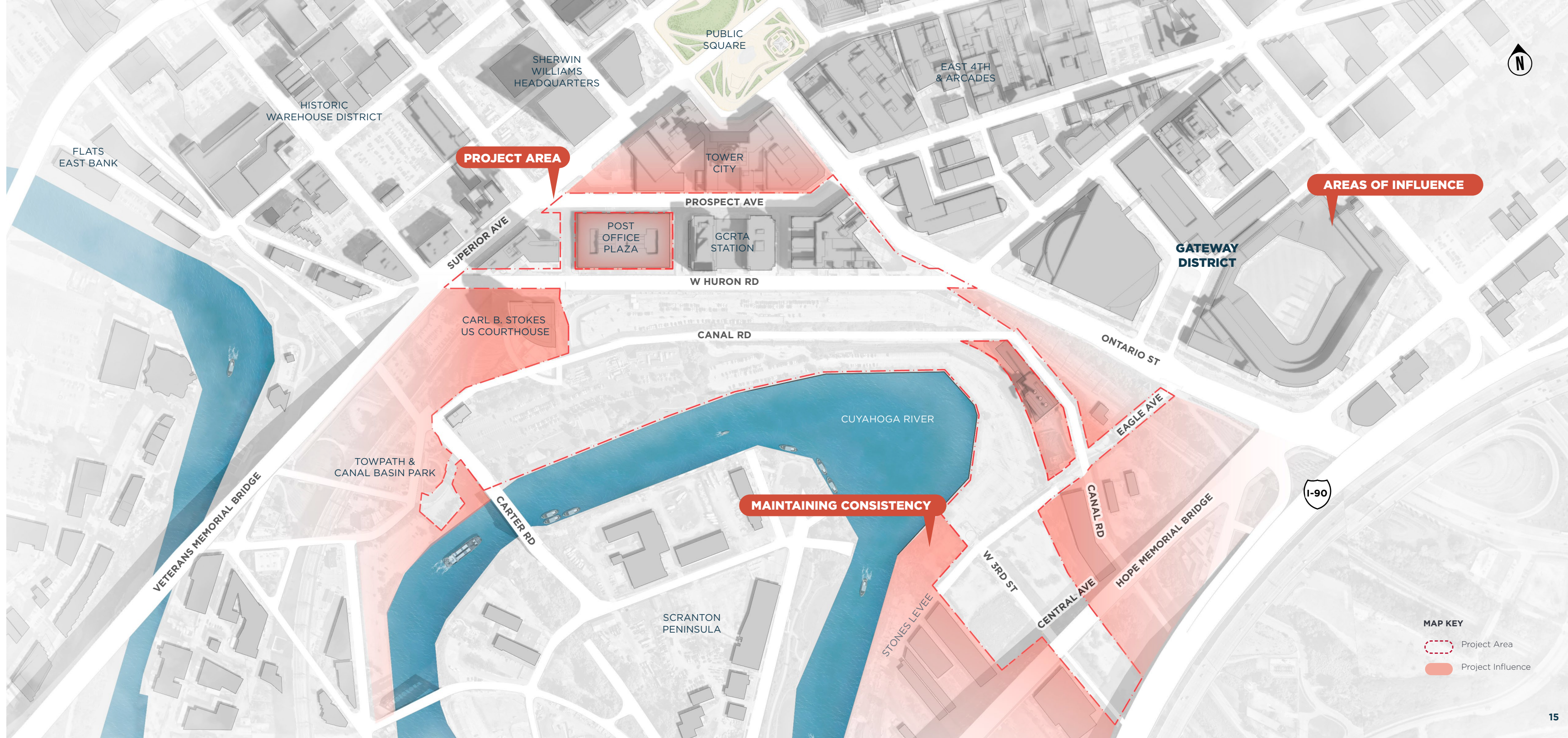
LOCATION

Project Influence

Ensuring Consistency and Predictability

Developing this district requires maintaining a consistent character, style, and urban form across all locations and phases. This framework should work to extend beyond the project area, ensuring that all public and private investments within or adjacent to the district reflect previous phases and the surrounding context.

Establish a baseline standard of quality and character for streets, parks, buildings, or the adaptive reuse of any property that may be underutilized.



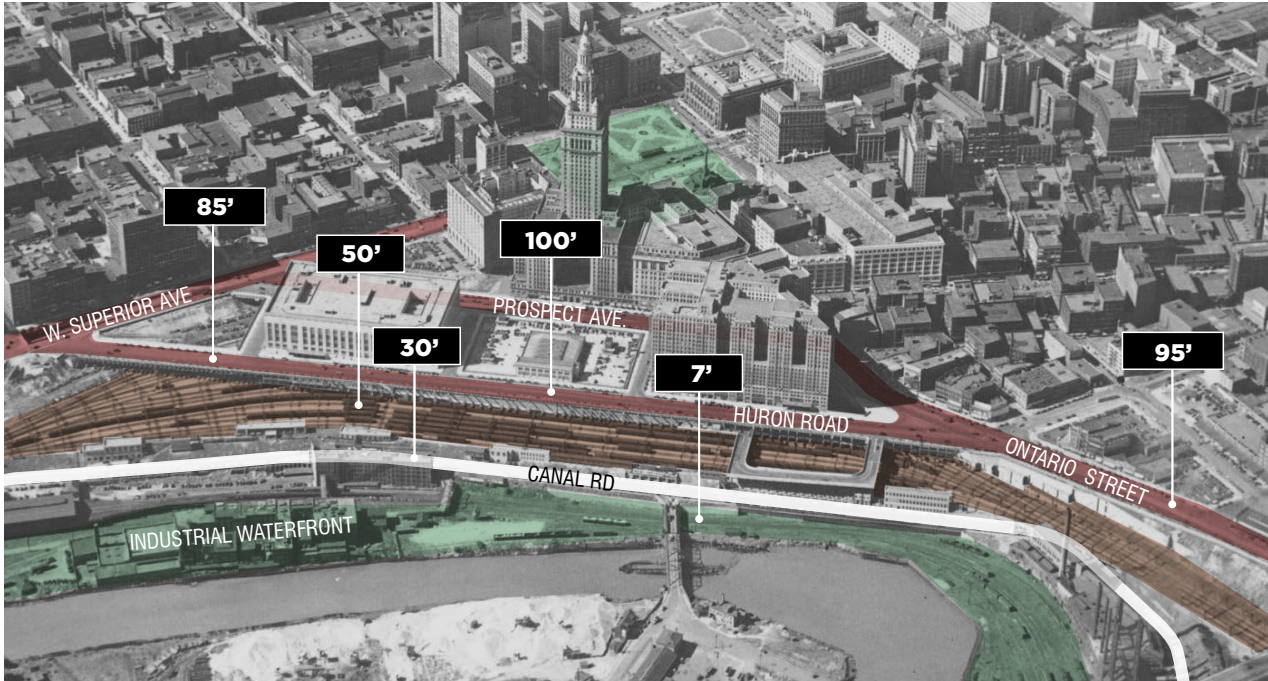
EXISTING CONDITIONS

Buildings + Parcels

Existing Conditions and Ownership

The project area, a former railyard converted to large expanses of surface parking for offices and entertainment, has undergone various physical and ownership transformations. Home to Sherwin Williams' original location, part of the company's footprint remains on site until the completion of the company's new downtown headquarters. Once vacated, this site, combined with all the remaining Bedrock-owned lower-level parcels, creates a unique opportunity to reconnect with the Cuyahoga River while supporting the adjacent buildings and central business district with new residences, parks, trails, and entertainment venues.

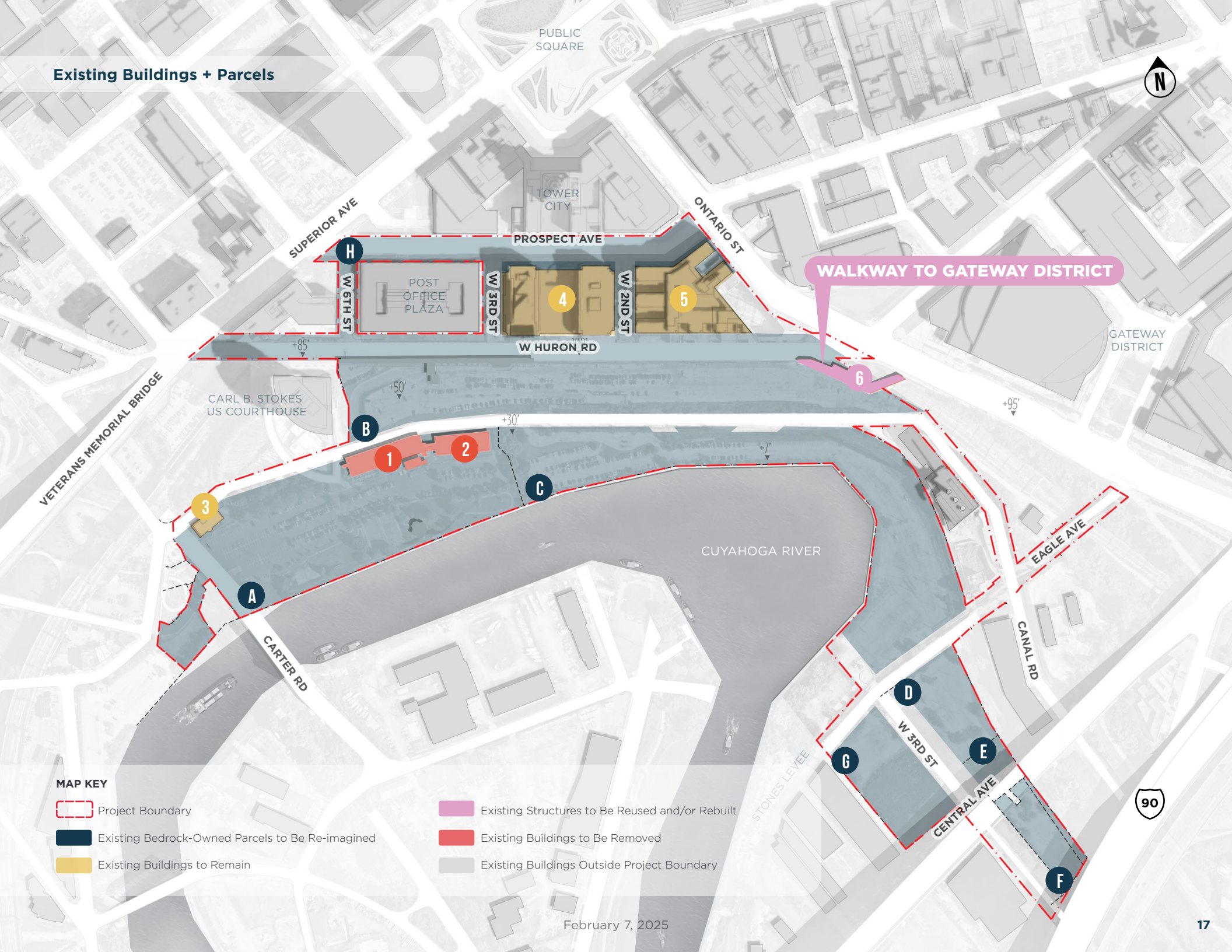
Historical Elevations Above River



Structure	Data
1	Sherwin Williams
2	Sherwin Williams
3	W&LE RR Depot (B&O)
4	Tower City Center
5	Landmark Office Towers
6	Walkway to Gateway

Parcel	Parcel #	Size (Acres)
A	101-20-023	9.6536
B	101-23-050A	8.5479
C	101-21-002	8.5687
D	101-30-003	1.4865
E	101-30-002	.3425
F	122-18-011 & 010	1.0785
G	101-30-001	2.1794
H*	101-30-050J*	7.7831
Total		~39.64 Acres

*Denotes Bedrock-Owned Commercial Parking Garage Parcel on Lower Levels



MAP KEY

- Project Boundary
- Existing Structures to Be Reused and/or Rebuilt
- Existing Bedrock-Owned Parcels to Be Re-imagined
- Existing Buildings to Be Removed
- Existing Buildings to Remain
- Existing Buildings Outside Project Boundary

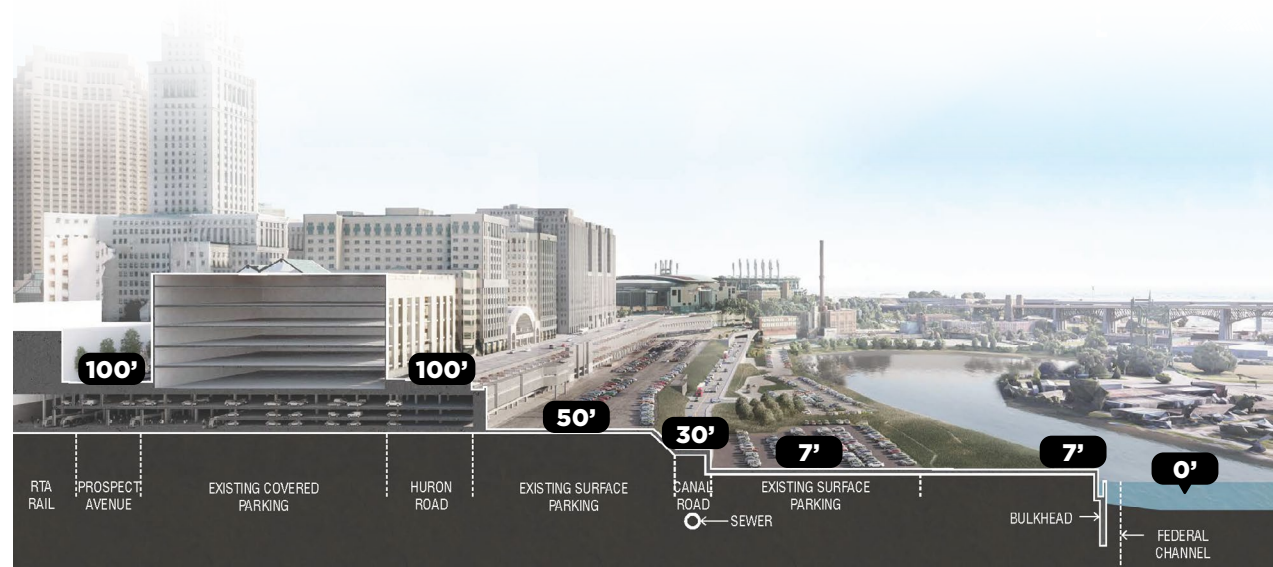
EXISTING CONDITIONS

Streets + Access

Street Network and Access

The Cuyahoga Riverfront Master Plan project area has two distinct roadway networks. The upper-level roads around the Terminal Tower complex (West Huron Road and Prospect Avenue) are integral to traffic flows and GCRTA bus operations in the Central Business District. The lower-level roads closer to the river, including Canal Road and West Third Street, currently serve entertainment businesses, manufacturing firms, and bulk material handling docks along the river.

Existing Elevations Above River



Rendering by Adjaye Associates

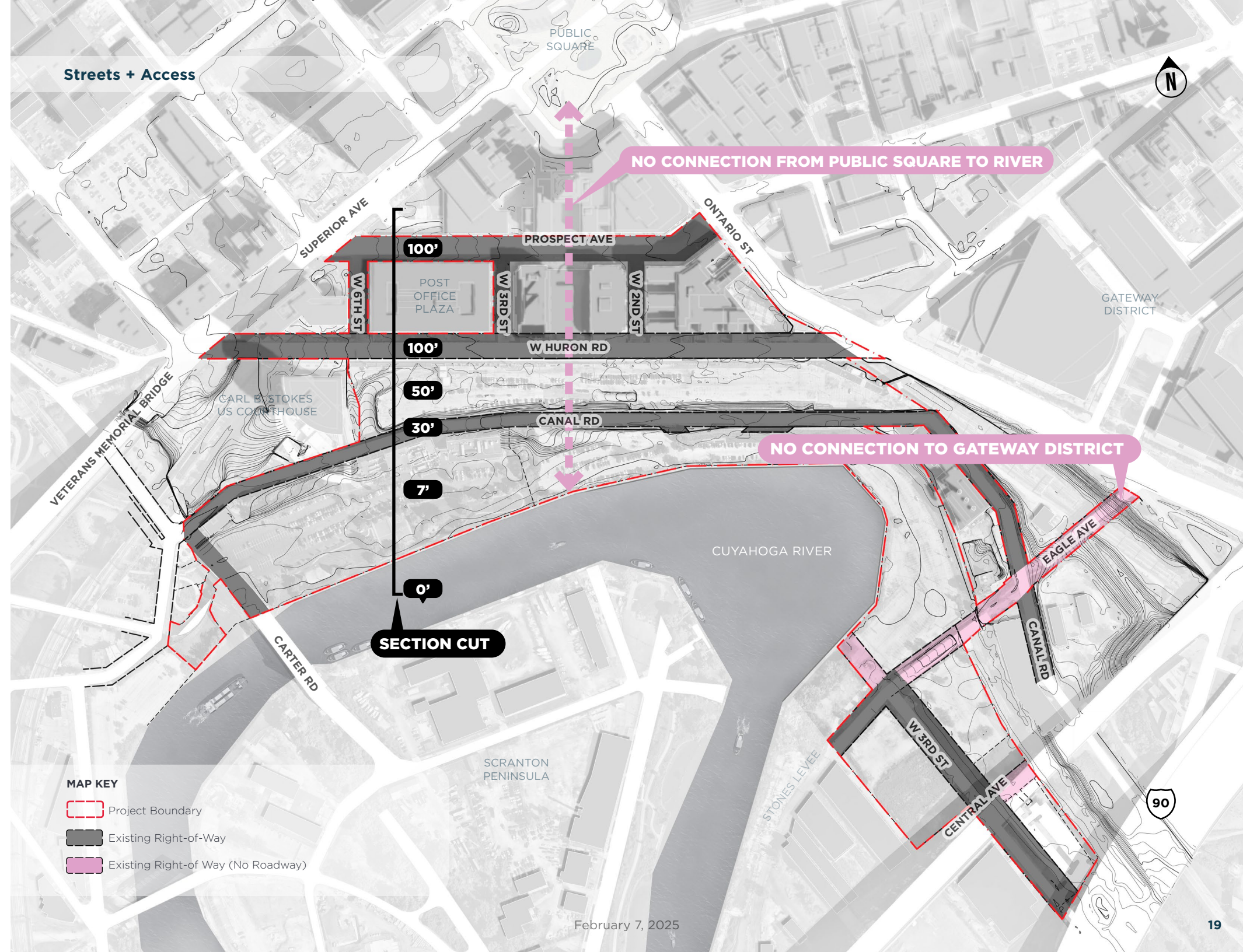
Street	ROW [^]	# of Lanes	Parking
Prospect*	100'	6 Lanes	Both Sides
Huron*	84'	6 Lanes	One Side
W. 6 th *	70'	3 Lanes	No Parking
W. 3 rd *	59'	4 Lanes ⁺	No Parking
W. 2 nd *	59'	4 Lanes	Both Sides
Canal	48' & 66'	2 Lanes	No Parking
Eagle	66' & 70'	Closed to vehicular traffic	
W. 3 rd (River Level)	70' & 103.5'	4 Lanes	One Side
Central	66'	Unimproved Right-of-Way	
Carter**	-58 ^{**}	2 Lanes	No Parking
Stones Levee	70'	2 Lanes	No Parking

[^]Current Right-of-Way Width

*Denotes roadway is currently on structure - see section

⁺Denotes one-way street

**Denotes estimated bridge deck width only - ROW unknown



Streets + Access

MAP KEY

- Project Boundary
- Existing Right-of-Way
- Existing Right-of-Way (No Roadway)

VISION

Objectives + Timeline

Cuyahoga Riverfront Master Plan Vision

The Cuyahoga Riverfront at Tower City Center will be a new downtown neighborhood where people can live, work, gather, and connect with the riverfront. Bedrock Cleveland, in collaboration with the City of Cleveland and other partners, is crafting a visionary plan spanning the next 15 to 20 years to revitalize 35 acres of the Cuyahoga Riverfront, transform Tower City Center, and bring to life the vision of a vibrant downtown that embodies a 15-minute neighborhood and is part of an 18-hour city.

The Cuyahoga Riverfront Master Plan is focused on mobility, accessibility, equity, opportunity, and sustainability. It aims to leverage the best features and strategies to place Cleveland at the forefront of innovative urban riverfront and transit-oriented development. The Cuyahoga Riverfront Master Plan represents a new era for Cleveland and contains a vision and recommendations that embody the spirit, culture, and character of Cleveland's next century.

Cuyahoga Riverfront Master Plan Objectives



Develop downtown Cleveland as a 15-minute neighborhood that positions Cleveland towards becoming an 18-hour city

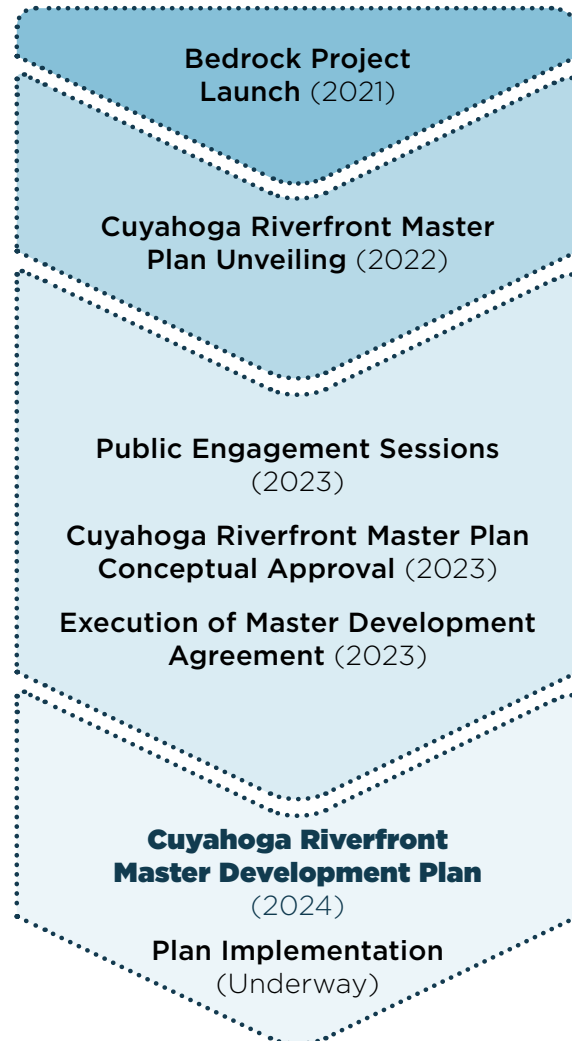


Ensure equitable delivery of economic growth and equitable residential, recreational, and mobility opportunities



Leverage sustainable infrastructure and public spaces as a driver for investment

Project Sequence and Milestones



Parks and Open Space

This future neighborhood will feature a mix of programming with parks, open space, pathways, and the riverfront at every turn. This plentiful and accessible green space along the river draws inspiration from the "Forest City." A Riverfront Park and Riverwalk and other pathways will weave together a variety of textured, cultured, and equitable public spaces that cater to diverse interests and ages. Park and open space projects may include:

1. Amphitheater and Hillside Knoll
2. Courtyard Steps and Cleveland Steps
3. The Porch and The Belvedere
4. Forest Hill
5. Kayak Launch and Plaza
6. Public Riverfront Park and Riverwalk

Transportation Infrastructure

The Cuyahoga Riverfront is envisioned as an economic district in downtown Cleveland that will result in transformational, development-ready parcels sharing public, sustainable, and resilient infrastructures. Distinctive features of this plan include links from Public Square to the riverfront, emphasizing and supporting downtown Cleveland's Shore-to-Core-to-Shore initiatives. These connections will be unifying features for this transit-oriented neighborhood by fostering links to the riverfront, transit, trails, commerce, culture, and community. Conceptual transportation infrastructure projects include:

1. Eagle Avenue Ramp/Bridge Construction
2. New Roadway Construction
3. Canal Service Road Extension
4. Streetscape Improvements
 - a. Prospect Avenue
 - b. Huron Road
 - c. West 6th Street
 - d. West 3rd Street
 - e. West 2nd Street
 - f. West 3rd Street (River Level)
5. Improved GCRTA Bus and Rail Access
6. Improved Regional Bicycle Network Access

Development Opportunities

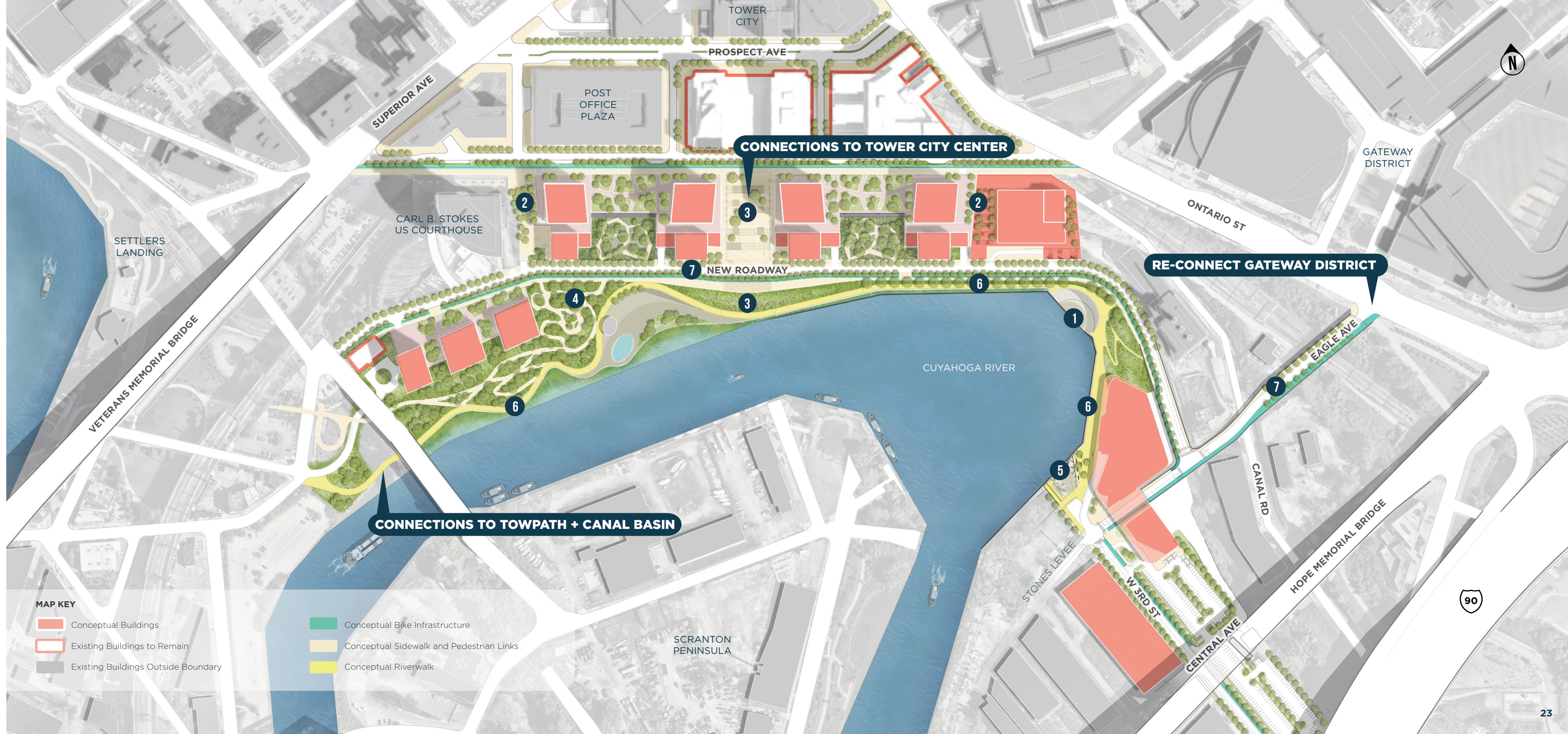
The Cuyahoga Riverfront Master Plan sets up a conceptual framework to guide downtown Cleveland toward becoming a diverse, mixed-use, mixed-income neighborhood by bolstering downtown's residential growth, business attraction, and entertainment venues through development incentives, community strategies, funding mechanisms, and equitable outcomes. Conceptual development opportunities include:

1. Cleveland Clinic Global Peak Performance Center + Cleveland Cavaliers Practice Facility
2. Entertainment and Hospitality
3. Mixed-Use Retail and Affordable and Market Rate Housing
4. Class A Office Space
5. Community Facilities

VISION

Neighborhood Features

- 1 Amphitheater and Hillside Knoll
- 2 Courtyard Steps and Cleveland Steps
- 3 The Porch and The Belvedere
- 4 Forest Hill
- 5 Kayak Launch and Plaza
- 6 Riverfront Park and Riverwalk
- 7 New Roadway + Eagle Avenue Bridge

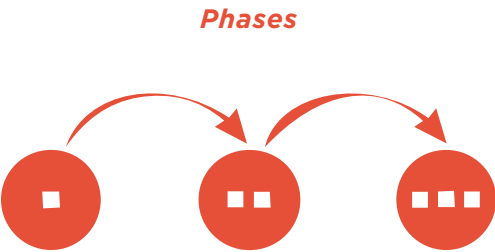


PHASING

Conceptual Sequencing

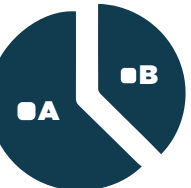
Proposed Stages and Projects

The Cuyahoga Riverfront Master Plan (CRMP) will be developed over a long period, which will include several phases. Each phase of the plan is further broken down into subsequent sequences. These sequences are more detailed in nature and represent a micro vs macro view of the project. They compose the day-to-day efforts of the development teams, which result in the submission of documents for City approval and construction contracts. While implementation is the goal of every plan, project sequencing is still guided by the conceptual vision and estimated phasing outlined in the CRMP. The phases and sequences shown here are estimated only and subject to changes based on market conditions, financing, and approvals.

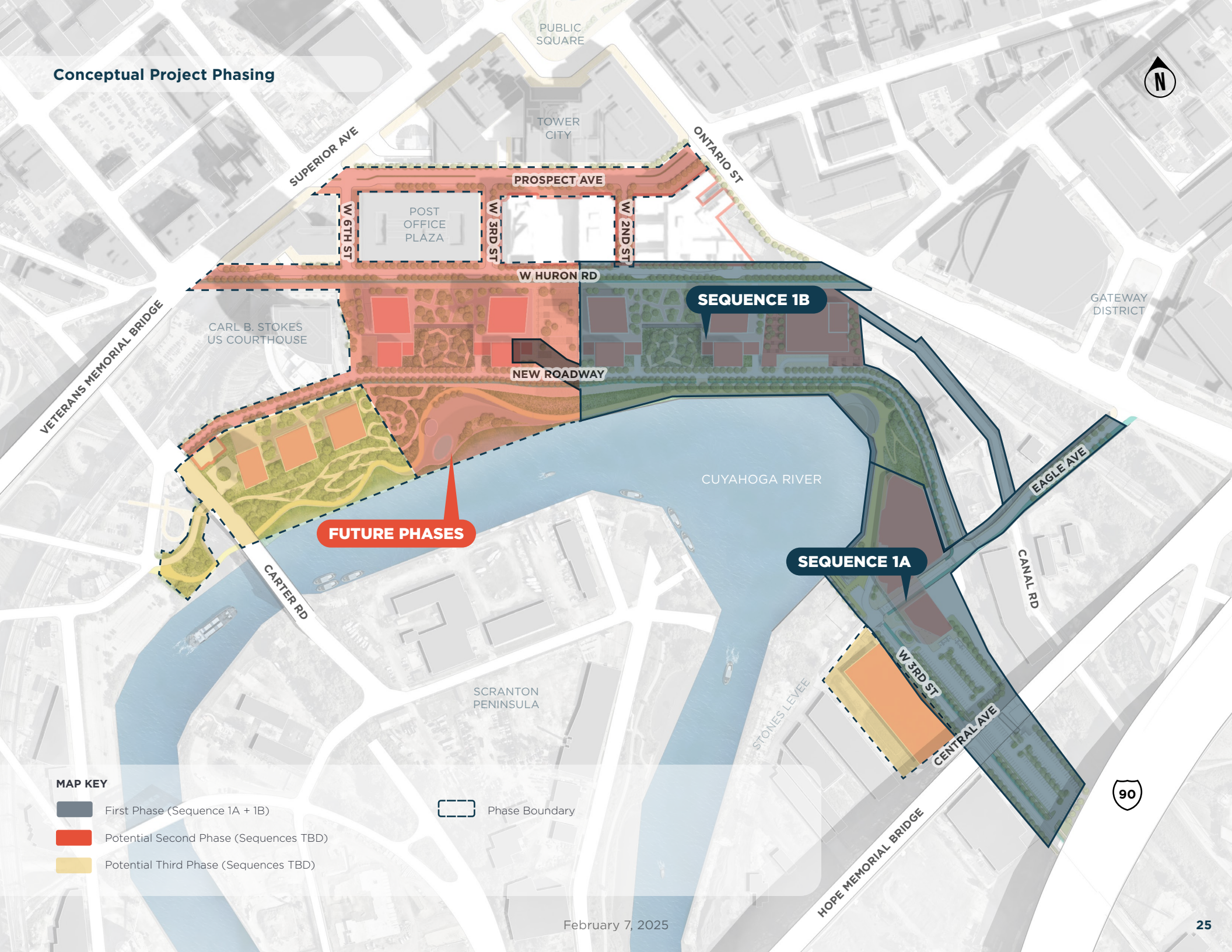


- Macro level view
- Longer and flexible timeline
- Evolving boundaries and projects
- Adapts to market conditions
- Conceptual design detail
- Conceptual cost and financing
- Conceptual order of operations

Sequences



- Micro level view
- Segmented breakdown of phase
- Shorter timeline
- Defined boundaries and projects
- Construction packages and design detail
- Detailed cost estimates and funding strategies
- Detailed order of operations
- Infrastructure and vertical development

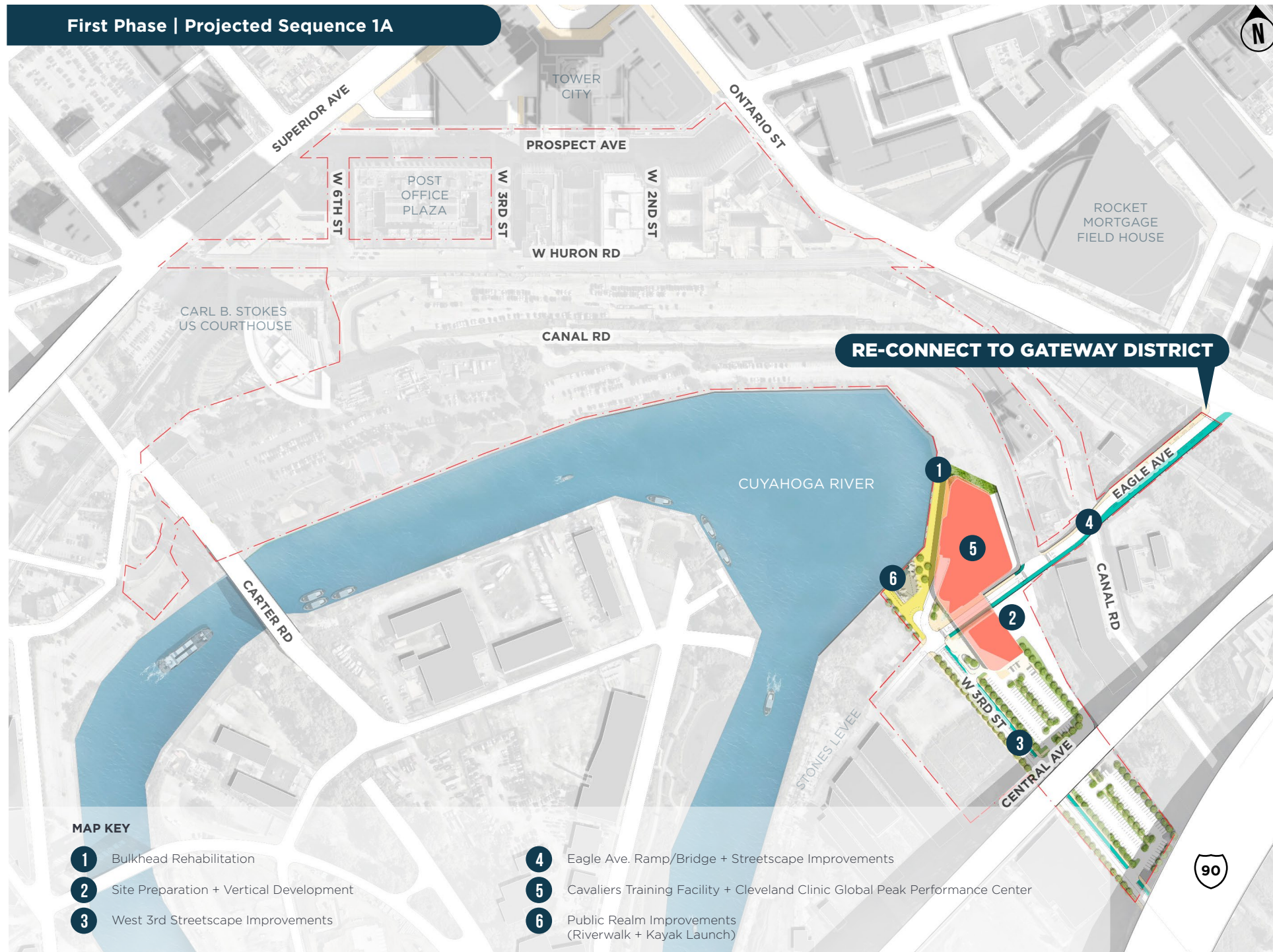


Conceptual Project Phasing

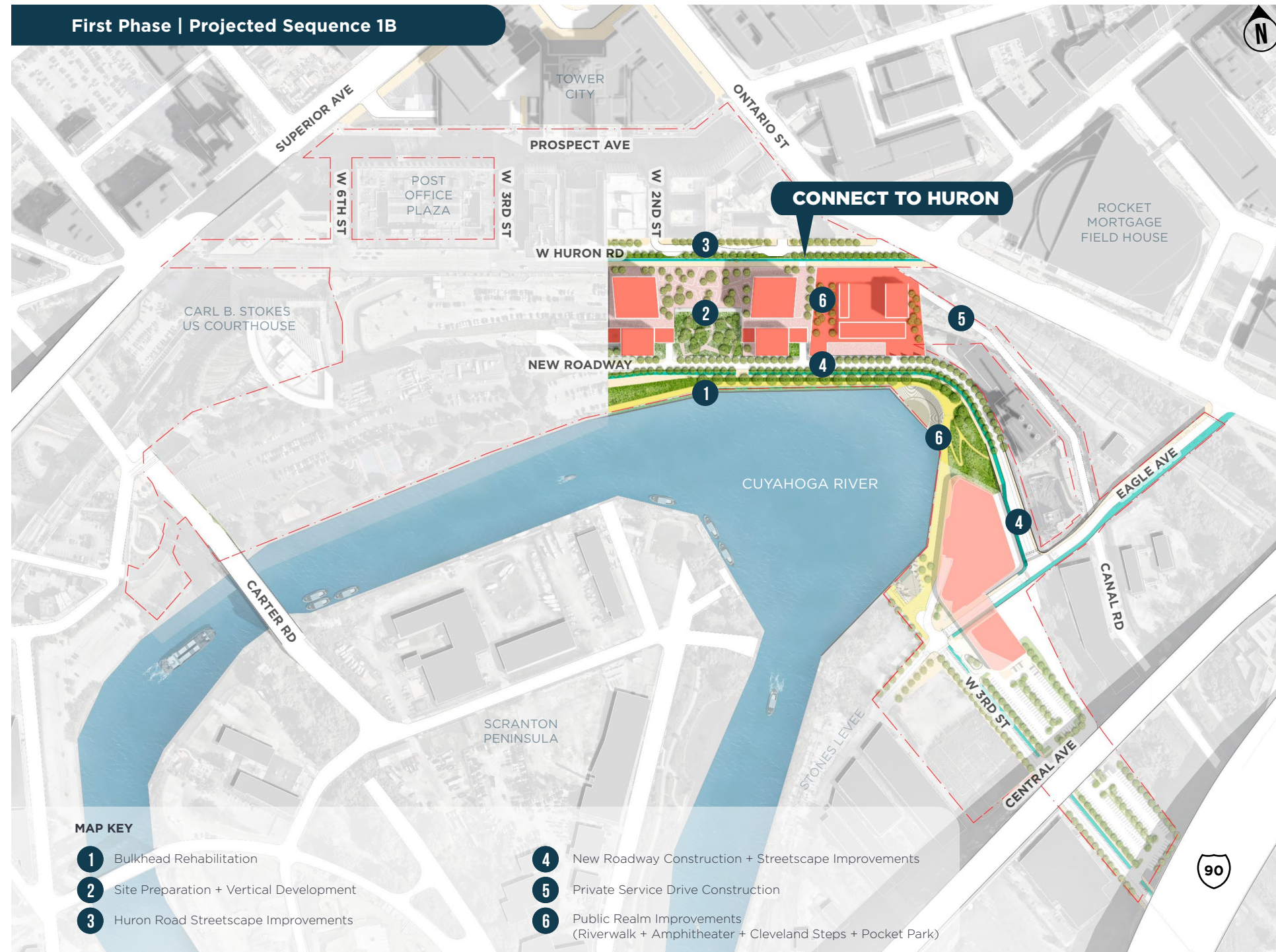
MAP KEY

- First Phase (Sequence 1A + 1B)
- Potential Second Phase (Sequences TBD)
- Potential Third Phase (Sequences TBD)
- Phase Boundary

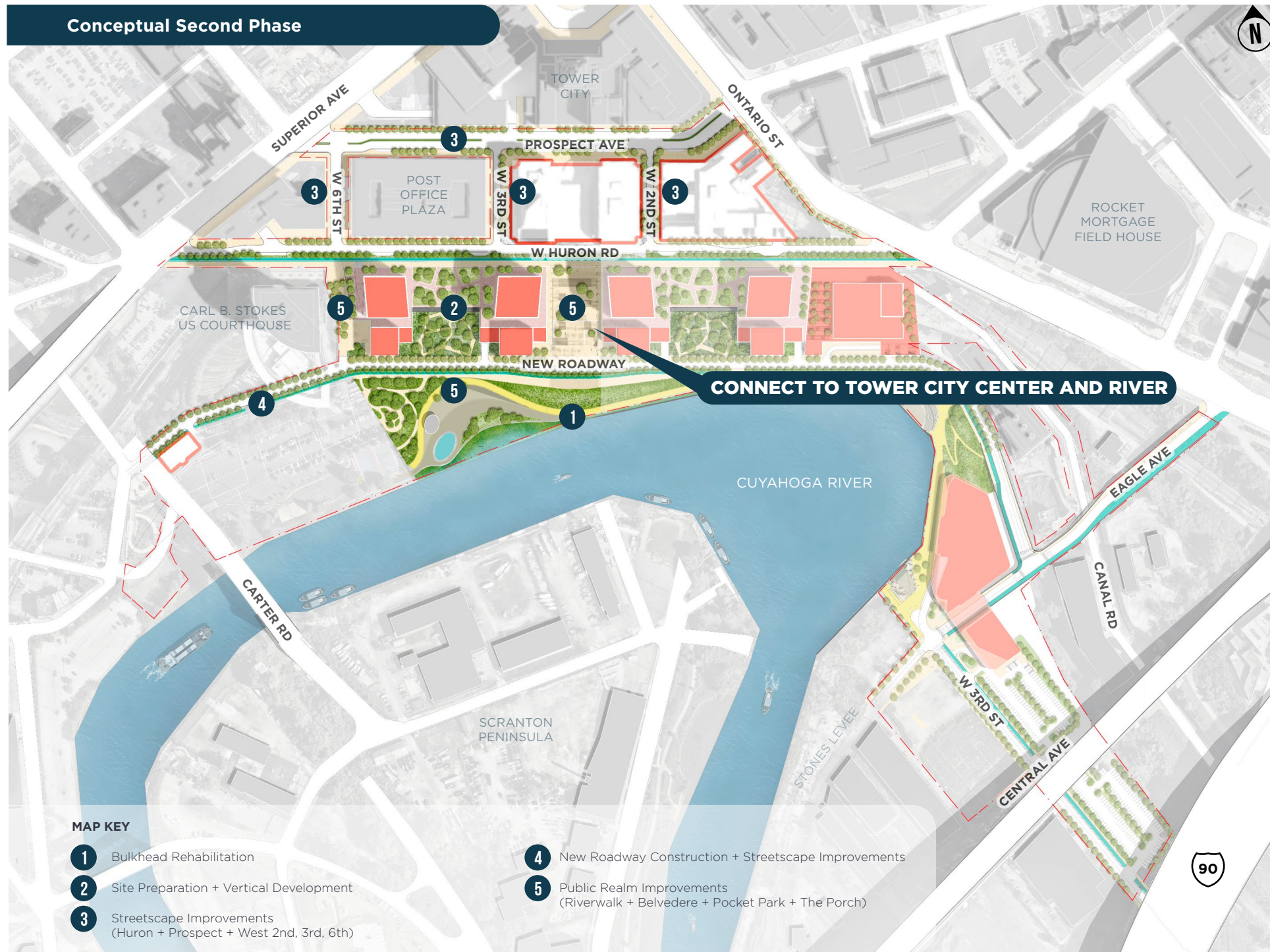
First Phase | Projected Sequence 1A



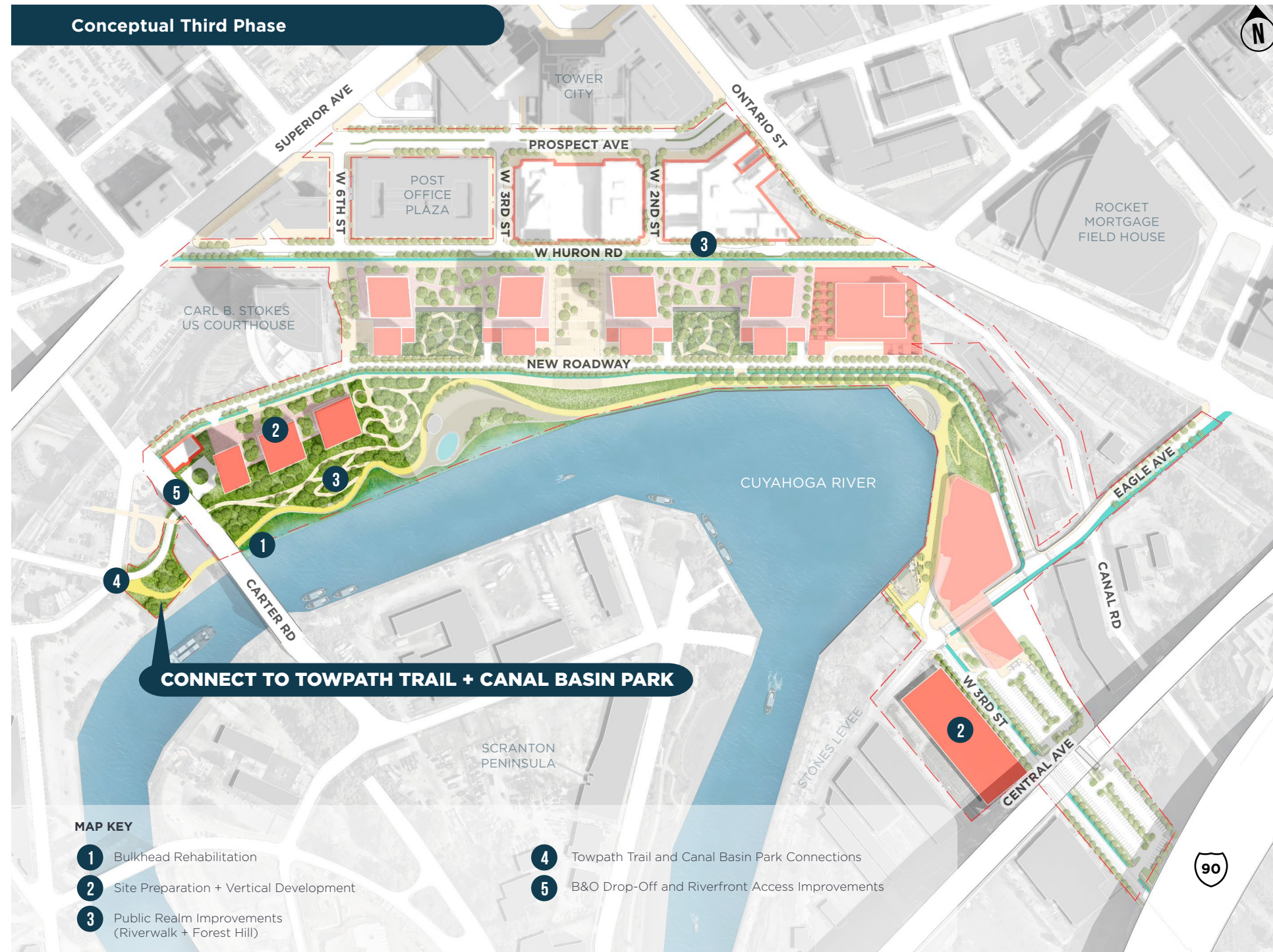
First Phase | Projected Sequence 1B



Conceptual Second Phase



Conceptual Third Phase



PURPOSE

Master Development Plan

From Vision to the Built Environment

Bedrock and its Cuyahoga Riverfront Master Plan team envision the Cuyahoga Riverfront project area as a new downtown neighborhood where Clevelanders launch careers, start families, attend events, take transit, walk, and play along the Cuyahoga River. This Master Development Plan (MDP) document represents the next step in advancing Bedrock's conceptual vision for this riverfront district. The role of the MDP is to provide a framework for developing buildings, parks, places, spaces, and streets. A complementary document to the Master Development Agreement (MDA), the MDP outlines guidelines for public infrastructure and private development to ensure that the Plan's conceptual vision is reflected throughout the built environment and that its objectives of cultivating open space, community, and equitable economic development are achieved through a phased approach utilizing public and private investments.

Evolving, Dynamic, and Adaptable

As the district grows and evolves, emerging technologies, contextual considerations, and alignment with previous phases or neighboring investments will increasingly shape the trajectory of the design and location of new riverfront park spaces and buildings. This holistic approach is intended to facilitate ongoing revisions to the Master Development Plan (MDP) document and allow greater flexibility in updating, interpreting, and implementing the plan for specific projects and zones within the district and neighboring areas. Consequently, it is imperative to regard this document as inherently fluid and open-ended in a way that enables the district's design to evolve dynamically in response to changing market conditions and the needs and aspirations of the community while striving to be creative and aspirational through innovative methods, features, and design elements.

A Guide to the Planning and Design of the Cuyahoga Riverfront

The Master Development Plan (MDP) is intended to be a flexible resource for developing this unique transit-oriented neighborhood. The MDP should serve as a resource for Bedrock, City leadership, stakeholders, and other regional organizations to ensure the character and amenities built through future public and private investments meet the goals and aspirations of the plan and the greater Cleveland community. The MDP intends to help achieve the following objectives:

1. Re-imagine a historically underutilized portion of the downtown Cleveland Riverfront and increase the overall character and attractiveness of the Cuyahoga River corridor as a premier neighborhood and destination within Cleveland.
2. Verify that all buildings, parks, streets, and other facilities within the Cuyahoga Riverfront Master Plan are planned, designed, and developed in an orderly, consistent, and high-quality manner.
3. Confirm that each project and building contribute in a complementary manner to the Cuyahoga Riverfront Master Plan and any future Riverfront or downtown Cleveland projects.
4. Deliver predictability and instill confidence that the conceptual vision and character will be consistent throughout the build-out—from the first project to the last.
5. Supply guidance for entities proposing or evaluating projects within or directly adjacent to the Cuyahoga Riverfront Master Plan project area.

PROCESS

Evaluation + Design Review

Addressing Quality and Compliance

Implementation of the Cuyahoga Riverfront Master Plan is intended to be collaborative, addressing each phase, sequence, or project quality and compliance with not only the Cleveland Planning Commission (CPC) submission requirements but also alignment with the objectives, principles, and vision defined by these guidelines.

When are Projects Reviewed

The CPC reviews all master plan projects during the conceptual, schematic, and final design phases. As part of the design review process, other reviewing bodies, including PETBoT (a committee of City staff representing Planning, Engineering, Traffic Engineering, and Bureau of Traffic and other public agencies) will also assess proposed designs. In addition to the CPC review, Bedrock may reserve the right to review and approve the design and construction of public art, buildings, and spaces within the Cuyahoga Riverfront Master Plan brought forth by other development teams. Refer to Design Review: A Guide for Applicants for the full extent of the Design Review Process and submittal requirements, including the Transportation Demand Management (TDM) Program checklist and other documents prescribed by the City of Cleveland. <https://planning.clevelandohio.gov/designreview/applicantsGuide.pdf>

Design Review Process

The Master Development Plan's (MDP) Guidelines are intended to be flexible and not all-encompassing. They only provide the Cleveland Planning Commission, Design Review Committees, City Staff, and other agencies with a general framework of recommendations or criteria to help assess the effectiveness or continuity of a particular project within the Cuyahoga Riverfront Master Plan during the design review process.

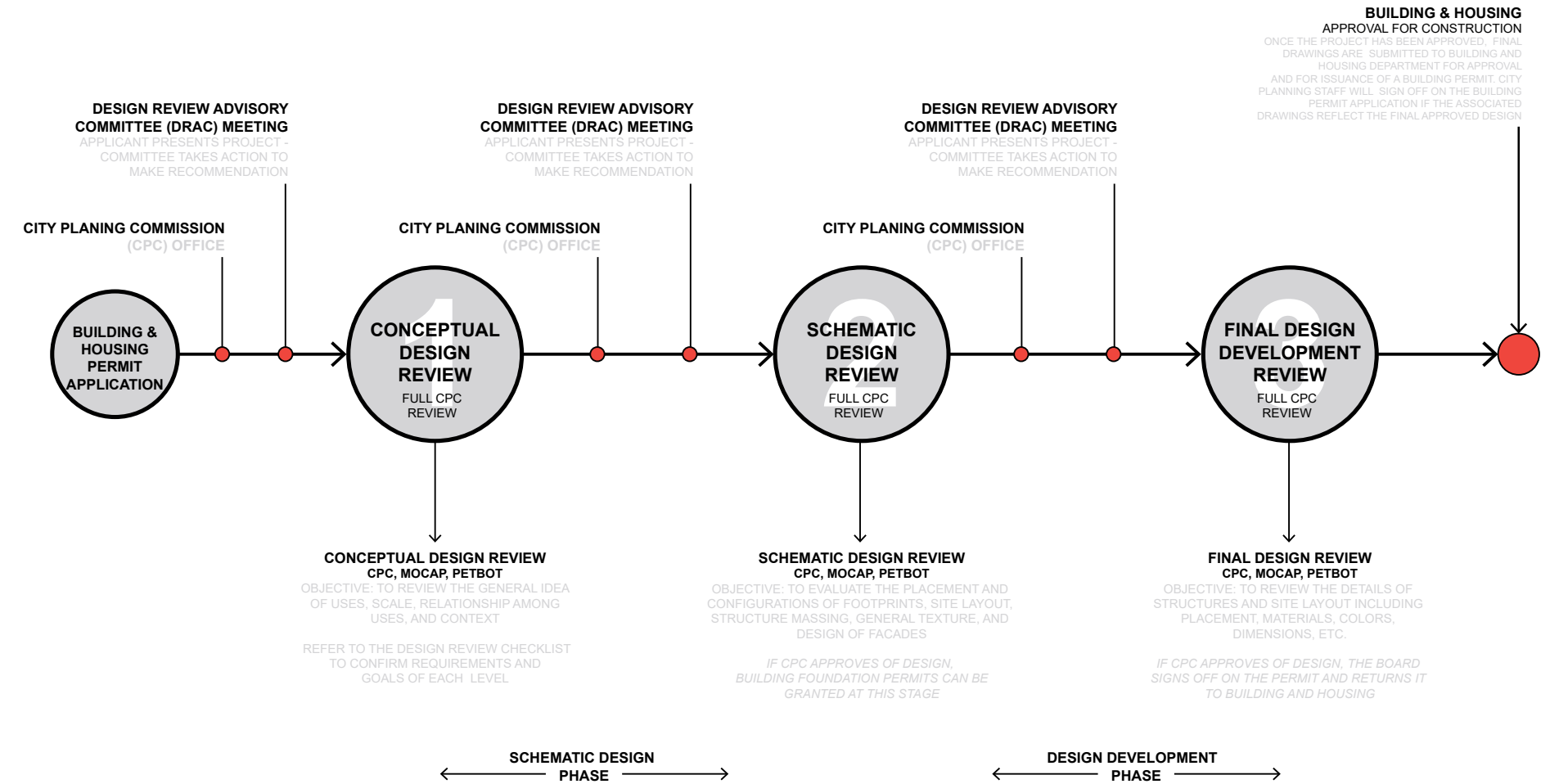
As each phase, project, or building is designed, it will undergo the typical review process prescribed by the Cleveland Planning Commission. Through the conceptual, schematic, and final design process, additional design details not contained in the MDP may be further evaluated by the City of Cleveland; this evaluation will include critical items such as:

- Design Quality
- Compliance with existing standards, codes, design guidelines, and regulatory requirements
- Consistency with public plans and policies and other approved or adopted planning documents
- Recognition of modifications and conditions highlighted by the Planning Commission and its representatives

Usage and Interpretation

Developer(s), Architects, Planners, Landscape Architects, city staff, and the Planning Commission may use these design guidelines as a reference for various projects and phases envisioned as part of the Cuyahoga Riverfront Master Plan. As the district vision is realized over time, the MDP and its guidelines should be treated as a living document that may require amendments to allow for changing technology, market factors, and other conditions not anticipated today. The recommendations, objectives, and strategies listed in this document are intended to provide flexibility in their application, allowing future projects the ability to be creative and aspirational, using innovative methods, features, and design elements to capture the spirit and intent of the Cuyahoga Riverfront Master Plan in unique and sustainable ways. This is particularly important considering the unique nature of development proposals and sites in downtown Cleveland and along the Riverfront. This document will help property owners, tenants, developers, and the community clearly understand the vision and expectations for development in and around the Cuyahoga Riverfront Master Plan project area.

Design Review Process



PRINCIPLES

Urban Design Standards and Strategies



I. Mobility: Pedestrians and Multimodal Connectivity

District and Regional Connectivity: Design streets and corridors that connect with existing roadway infrastructure to support regional and local access to downtown and the Cuyahoga Riverfront. Provide the space necessary to integrate each existing modal network seamlessly into the street design and create safe access for all road users to and from the project area.

Pedestrians and Bicycle Facilities: Ensure street design and amenities for active transportation users are accessible and convenient within the streetscape and open spaces. This creates a safe network for all users and mobility types, regardless of age or ability. A pedestrian-scaled and accessible system of sidewalks and trails should traverse the district, providing convenient access to businesses, residences, and parks along the riverfront and between the Cuyahoga River and Public Square.

Seamless Mode Change: Leverage the proximity to Tower City Center and Public Square GCRTA stations to support multiple modes of travel in a single trip. Provide the connections necessary for safe and convenient change of modes between transit to walking or biking.

Parking and Service Access: Parking and service access should minimize the impact on mobility, streets, and the public realm. Where possible, separate building service access from other district circulation, screen parking garages, and lots, and use below-grade parking.

II. Streetscapes: High-Quality District Character

Balance Roadway Users and Zones: The design of the public right-of-way should address all features necessary to create a dynamic public realm. Establish adequate zones for all modes of travel to serve the community's current and future needs, including flexible and dedicated zones for pedestrians, bicycles, transit, and vehicles.

Pedestrian-Scaled Design and Access: Street design should be on a human scale and prioritize pedestrian circulation, with unobstructed movement that is high-quality, safe, comfortable, and accessible for all.

Activated Sidewalks: Encourage a vibrant public realm through outdoor dining spaces; transparency along ground-floor facades of buildings; and direct connections to open spaces, plazas, and the Cuyahoga Riverfront.

Public Realm and Placemaking: Establish a distinct and unified district identity along public streets and public open spaces through a network of paving, materials, landscapes, furnishings, wayfinding, and branding.

Green Streets and Landscaping: Design the public realm with a tree canopy that creates a comfortable, shaded environment. Integrate additional green infrastructure methods into the streetscape to capture stormwater, reduce the district's environmental impact, and support Cleveland's Complete and Green Streets regulations.

III. Open Spaces: Flexible, Accessible, and Ecological

Diversity and Quality: Diversify open spaces' size, design, and programming to encourage various park types and functions. Parks and open spaces, including the Cuyahoga Riverfront, should be conveniently accessible to district visitors and residents of northeast Ohio. All parks and open spaces, including public and private spaces, should maintain a distinct character and identity while allowing for creativity and differentiation through design and access.

Interconnected: Ensure that the open space system within the district is connected, integrated with regional networks and contributes to the overall district character, while providing opportunity for individual design and unique experiences.

Multi-Functional: Design open spaces for flexibility to accommodate a range of users, programming, and activation. Open spaces should be used in multiple ways across all seasons; this may require certain locations to coordinate with building programming or include basic necessities to support public use, such as restrooms, food operations, water fountains, play areas, and accessible parking.

Equitable: Open spaces within the Cuyahoga Riverfront district are distributed throughout the project area to ensure all buildings have access to nearby amenities. With Tower City Center at the core of the project, all members of the community—regardless of their background, location, socio-economic status, age, or ability—can gain access to the open spaces and the Cuyahoga Riverfront.

IV: Buildings A Neighborhood of Lasting Value

Activated Ground-floors: Anchor key corners and corridors with active commercial/retail uses that are oriented in a way that contributes to/interacts with the public realm. Create a contiguous building frontage to reinforce the street wall and define the public realm.

Diversity + Equity of Housing: Provide multiple housing options to accommodate a diverse population of residents in the district from families to individuals and a variety of price points from affordable to market-rate units.

Scale + Articulation + Transition: Ground-floor articulation, scale, and materiality should create a welcoming street environment and reinforce pedestrian scale. Design outer edges of the development to be as compatible with adjacent character as possible.

Sustainability: Building design and material selection should work to reduce the city's carbon footprint, while site preparation focuses on reusing brownfields and where possible addressing the project areas legacy of contamination.



SUSTAINABILITY

Resilient + Ecologically Responsible Neighborhood

Planning for Current and Future Residents

Sustainable planning, urban design, and architectural principles guide and influence all phases of the Cuyahoga Riverfront Master Plan. Green building practices inform design decisions, helping to position the district as a leader in reducing pollution and energy use, a promoter of transit and mobility, a supporter of clean energy, and a user of renewable and long-lasting building materials. Sustainable strategies improve individuals' life, work, and play experiences and help leverage additional public and private investments that protect Cleveland's historic, cultural, and environmental resources. Projects are to be constructed using green building practices to reduce impacts on resource consumption, ecological systems, and human health. These recommendations, built around achieving carbon neutrality, reducing building energy use, conserving water, and improving ecosystem services, help the community create a better balance between growth and the natural systems supporting it.



The public infrastructure and architectural guidelines chapters outline how the Cuyahoga Riverfront Master Plan can become sustainable through compact mixed-use transit-oriented developments, high-density land uses, public transit, multimodal infrastructure, and stormwater management via parks and green infrastructure. Building recommendations include utilizing energy-efficient technologies and renewable building materials that balance initial investments against the product's durability, long-term environmental impacts, and life cycle costs. Efficient building envelopes, use of natural light and high-performance glass, energy-efficient mechanical and lighting systems, water conservation technologies, stormwater management techniques, and diversion of waste disposed of in landfills are examples of sustainable design recommendations that mirror the project's values and commitment to environmentally conscious practices.

Existing Guidelines

The City of Cleveland continues establishing a comprehensive set of climate-friendly goals and policies across various departments and programs. These goals emphasize integrating sustainability into the community's bureaucratic processes and decision-making responsibilities. The City of Cleveland envisions itself as an emerging leader in sustainability and the freshwater economy. Several standards, programs, and resources have been adopted to help the City reach these goals including, the City of Cleveland's Transportation Demand Management (TDM) Program, the Complete and Green Streets Ordinance, 2024 Updated City of Cleveland Climate Action Plan and the Circular Cleveland Roadmap, and the 2030 City of Cleveland Climate Action Plan. These materials and future efforts should be referenced and utilized by all projects in the Cuyahoga Riverfront Master Plan to ensure their sustainable design and construction efforts adequately align with the community's goals and values.

Desired Goals and Objectives

- Achieve LEED v4.1 Cities and Communities Silver Certification for the entire Cuyahoga Riverfront Master Plan project area.
- Strive to achieve certification in LEED, Edge, SITES, or equivalent green building rating systems for individual buildings, parks and other structures that at a minimum meet the same standards including in city policies such as the Sustainable Building Policy.

Cuyahoga Riverfront Master Plan Low Carbon + Low Impact Initiatives



SUN SHADE: NATURAL LIGHT



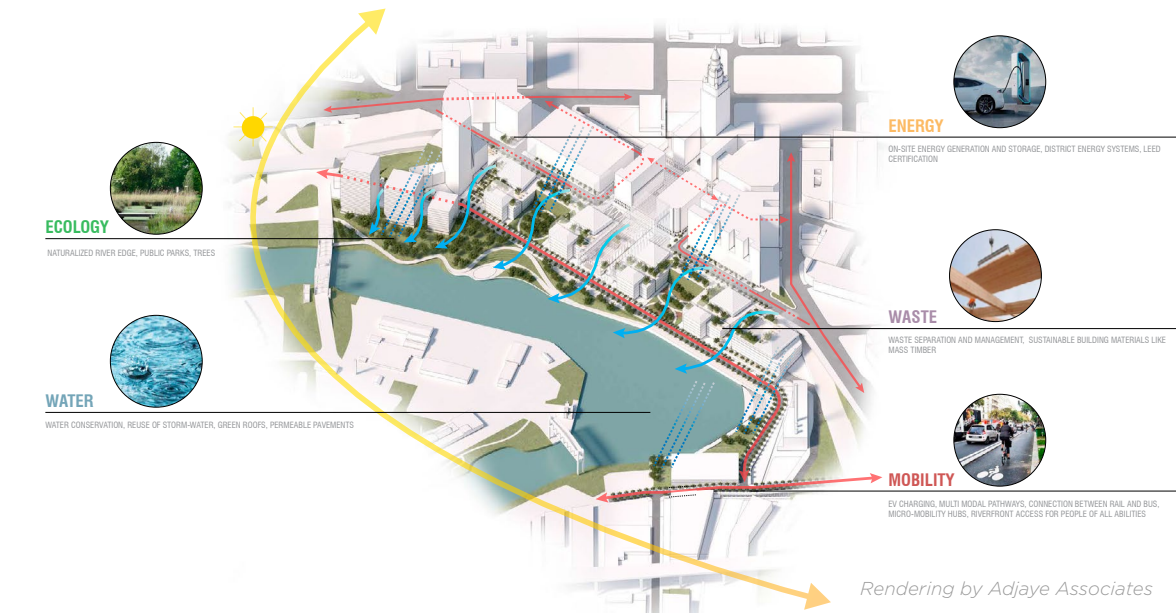
MASS TIMBER: RENEWABLE MATERIAL



ALTERNATIVE ENERGY



GREEN ROOFS



Rendering by Adjaye Associates



LEED for Communities: Plan and Design

CERTIFIED: 40-49		SILVER: 50-59		GOLD: 60-79		PLATINUM 80+	
Y	N	Y	N	Y	N	Y	N
64	4	0	42	SCORECARD TALLY			
				VERSION DATE: December 3, 2024			
2	2	0	1	INTEGRATIVE PROCESS (IP)			POSSIBLE: 8
Y				p1	Integrative Planning & Design Process		5
2	2	1	1	c1	Green Building Policy & Incentives		5
5	0	0	8	NATURAL SYSTEMS & ECOLOGY (NS)			POSSIBLE: 13
Y				p1	Ecosystem Assessment		
Y				p2	Construction Activity Pollution Prevention		
Y				p3	Green Spaces (v2.2024)		
5				c1	Natural Resources Conservation & Restoration		5
				c2	Light Pollution Reduction (v2.2024)		2
				c3	Resilience Planning		6
18	0	0	0	TRANSPORTATION & LAND USE (TR)			POSSIBLE: 18
6				c1	Compact, Mixed Use & Transit Oriented Development (v2.2024)		6
4				c2	Walkability & Bikeability		4
2				c3	Access to Quality Transit		2
2				c4	Alternative Fuel Vehicles		2
2				c5	Smart Mobility & Transportation Policy		2
2				c6	High Priority Sites RP		2
2	0	0	10	WATER EFFICIENCY (WE)			POSSIBLE: 12
Y				p1	Integrated Water Management		
Y				p2	Water Access & Quality		
5				c1	Stormwater Management RP		5
5				c2	Wastewater Management		5
2				c3	Smart Water Systems		2
4	0	0	0	REGIONAL PRIORITY (RP) ZIP: 44113			POSSIBLE: 4
1				c1	Option 1: LEED BD+C - High Priority Site		1
1				c2	Option 1: LEED BD+C - Surrounding Density & Diverse Uses		1
1				c3	Option 1: LEED ND - Case 1: Building Reuse		1
1				c4	Option 1: LEED ND - Reduced Parking Footprint		1
15	2	0	14	ENERGY & GREENHOUSE GAS EMISSIONS (EN)			POSSIBLE: 31
Y				p1	Power Access, Reliability & Resiliency		
9	2		8	p2	Energy & Greenhouse Gas Emissions Management (v2.2024)		19
4				c1	Energy Efficiency		4
				c2	Renewable Energy RP		6
				c3	Low Carbon Economy		-
2				c4	Grid Harmonization (v2.2024)		2
4	0	0	7	MATERIALS & RESOURCES (MR)			POSSIBLE: 11
Y				p1	Construction & Demolition Waste Management		
Y				p2	Solid Waste Management		
				c1	Organic Waste Treatment		2
2				c2	Recycling Infrastructure (v2.2024)		5
2				c3	Responsible Sourcing for Infrastructure		2
				c4	Smart Waste Management Systems		2
8	0	0	2	QUALITY OF LIFE (QL)			POSSIBLE: 10
Y				p1	Demographic Assessment		
Y				p2	Social Infrastructure		
				p3	Economic Growth (only LEED for Cities)		
2				c1	Affordable Housing		2
6				c2	Public Health		6
				c3	Emergency Management and Response		2
6	0	0	0	INNOVATION (IN)			POSSIBLE: 6
1				c1.1	EP: Comprehensive Mobility Plan		1
1				c1.2	Housing & Jobs Proximity		1
1				c1.3	Green Urban Development Education		1
1				c1.4	Tree-lined and Shaded Streetscapes		1
1				c1.5	Solar Orientation		1
1				c1.6	Community Benefits Agreement		1



2.

GUIDELINES FOR PUBLIC INFRASTRUCTURE

2



OVERVIEW

Guidelines for Public Infrastructure

I. Framework: Readyng the Site

- Bulkhead Rehabilitation
- Site Readiness and Restoration
- Eagle Avenue Bridge/Ramp
- New Riverfront Boulevard
- Canal Roadway Conversion

II. Program: Development Potential

- Strengthen the Urban Core
- Mix of Private and Public Uses
- Publicly Accessible Riverfront
- High-Density Urban Development
- Create a framework for sustained and lasting investment

III. Parks + Open Spaces: Diverse and Accessible

- Embrace the River
- Riverfront Parks and Riverwalk
- Publicly Accessible Spaces
- New Multimodal Links and Access

IV. Transportation: Transit Centered and Multimodal

- Transit Focused
- Maintain Vehicular Access
- Create a Walkable Environment
- All Ages and Abilities Access
- Bicycle and Micromobility Options

V. Pubic Art + Signage: Enriching, Enhancing, and Engaging

- Branding and Identity
- Distinct and Legible
- Cultural History and Character
- Activation and Storytelling

VI. Streetscapes: Landscaped, Active, and Pedestrian Scaled

- Create Safer Streets for All Users
- Dynamic and Multimodal
- Distinct Network of Landscapes and Furnishings

FRAMEWORK

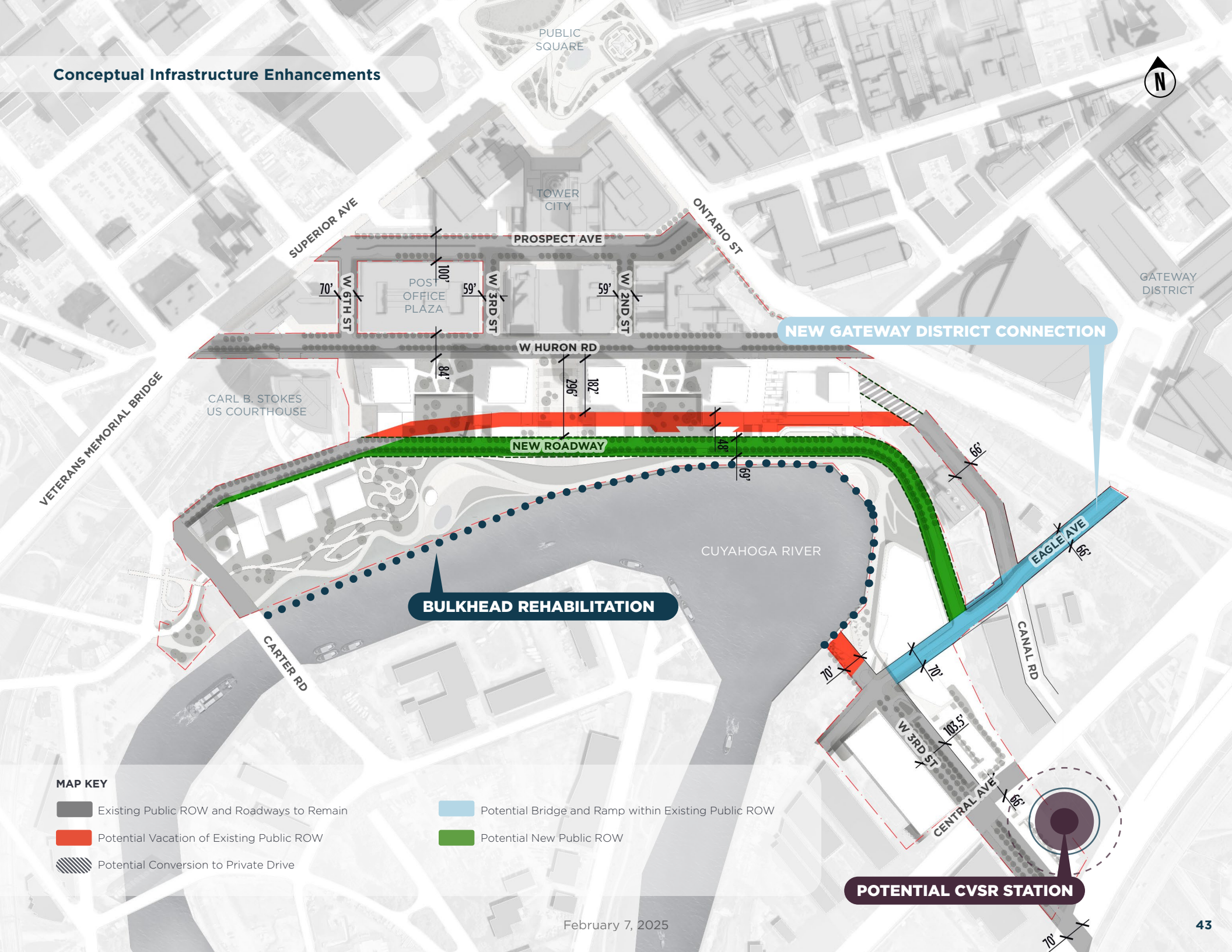
Big Moves

Readying the Site for the 21st Century

The Cuyahoga Riverfront Master Plan (CRMP) is designed to fit within its surroundings and link Cleveland's Core to the Shore, including the Cuyahoga Riverfront and the Central Business District. To establish this, a framework of several critical infrastructure 'big moves' are necessary to unlock the site's potential and prepare it for a 21st-century mixed-use development. These investments include a conceptual new riverfront boulevard (New Roadway), enhancements to existing project area roadways, Cuyahoga Riverfront Bulkhead Rehabilitation, and a New Eagle Avenue Bridge/Ramp reconnecting the Gateway District with the riverfront. These comprehensive investments set the foundation for the new parks, plazas, open spaces, multimodal connections, and development-ready construction pads outlined.

Framework Objectives:

- Ensure public and private infrastructure investments align with existing roadways and support the CRMP's vision.
- Leverage investments in infrastructure as catalysts and drivers for additional public investment and private development.
- Rehabilitate riverfront bulkheads adjacent to the property to support commercial shipping, new development, riverfront parks, and public access.
- Shift Canal Road south to create a new multimodal riverfront boulevard (New Roadway) and provide additional space to the north for new development adjacent to Huron Road and Tower City Center.
- Maintain existing Canal Road for delivery, service, and parking access for existing Tower City Center and future Development
- Reestablish Eagle Avenue and the city street-grid network as a direct connection from the Gateway District to the Cuyahoga Riverfront.
- Support ongoing and future infrastructure efforts that may align with the master plan vision including opportunities to extend the Cuyahoga Valley Scenic Railroad (CVSR) to downtown Cleveland.
- Explore opportunities to create new pedestrian connections between Huron Road and New Roadway that provide direct access between the Cuyahoga River and Public Square.

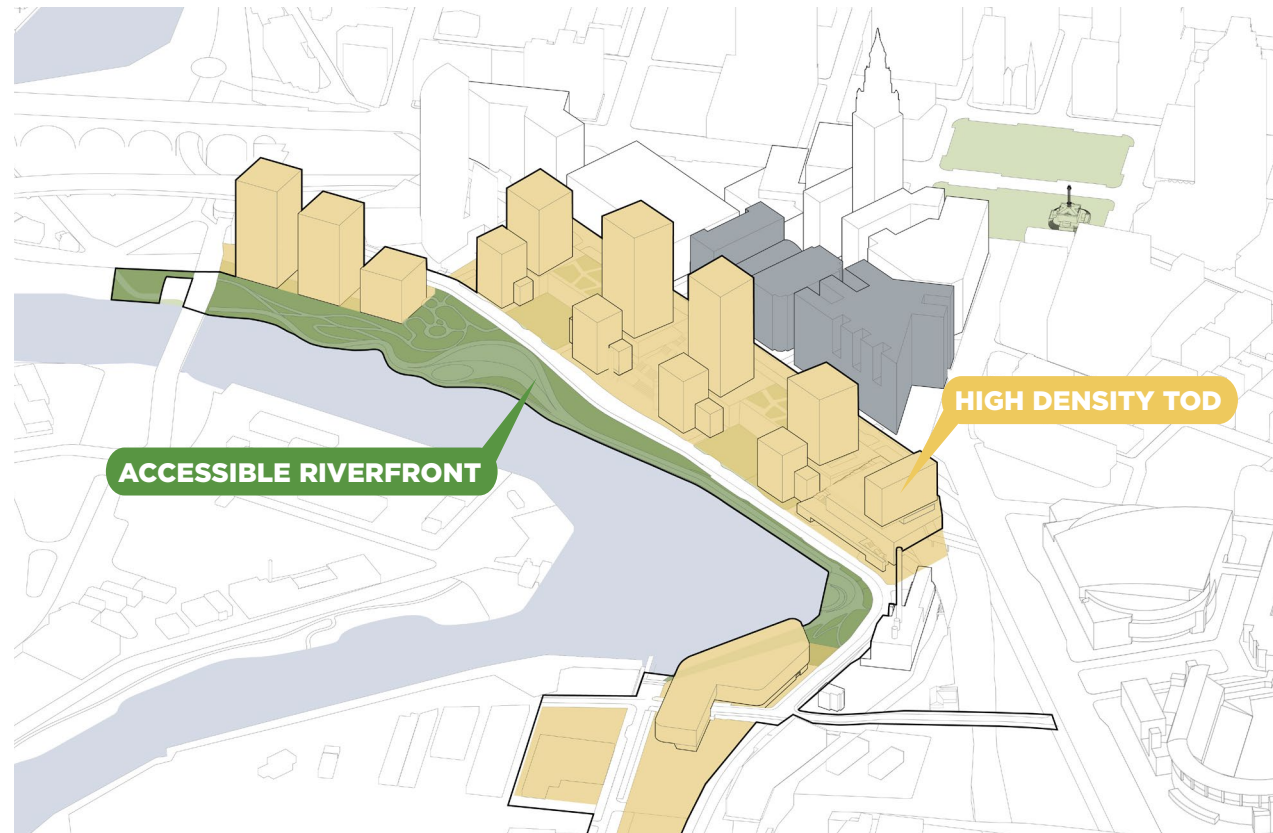


PROGRAM

Land Uses

Unlocking the Project Area's Potential

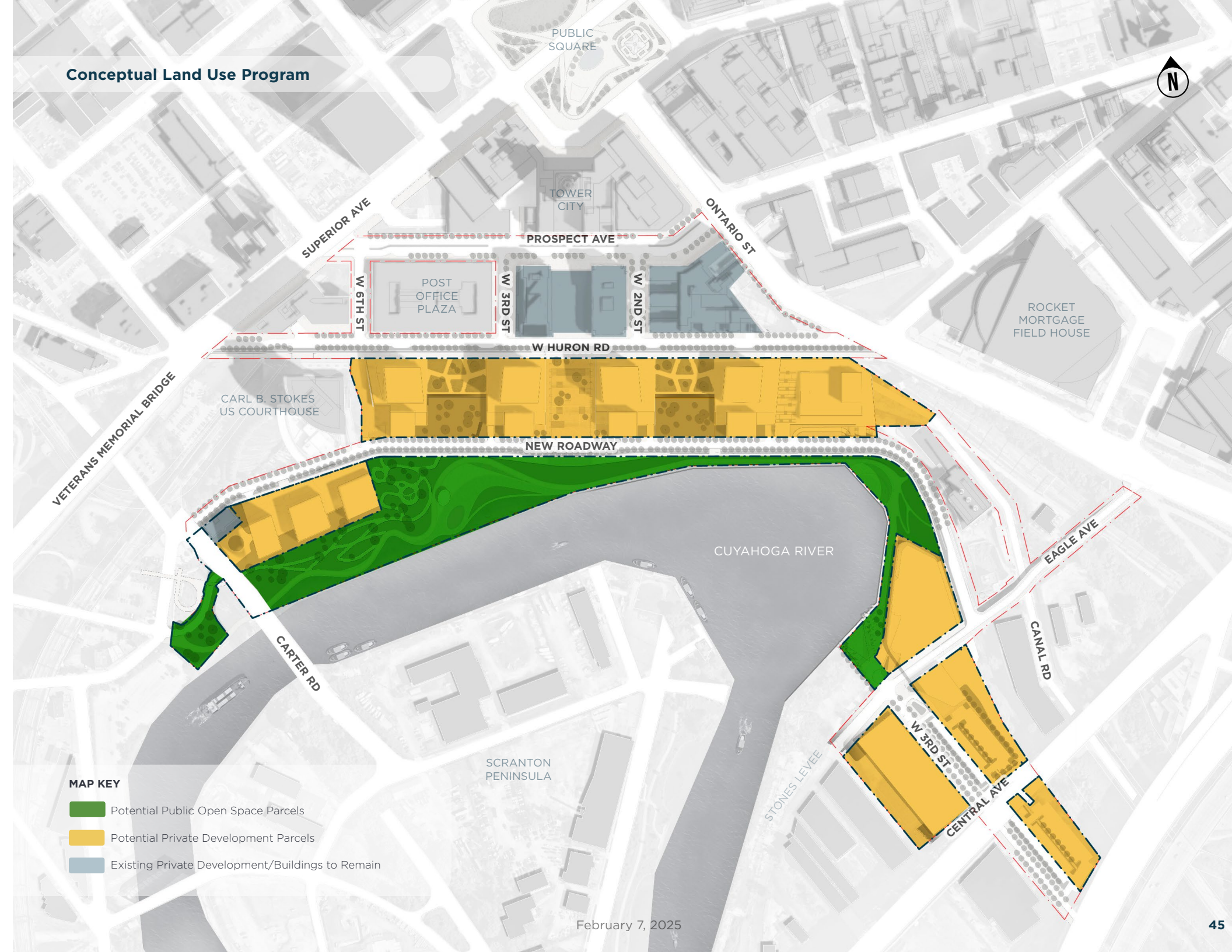
This comprehensive effort seeks to establish a baseline for the desired character and feel of the neighborhood, including the location of proposed open spaces, parks, adaptive reuse of existing buildings where appropriate, and future development. The Cuyahoga Riverfront Master Plan emphasizes transit, connectivity, river access, and a phased development approach linked to the Tower City Center complex and Public Square. The plan centers around a continuous Riverfront Park and Riverwalk, serving as a physical anchor for the project area, linking future phases and connecting the Central Business District and Clevelanders to the Cuyahoga River.



Land Use Objectives:

- Incorporate publicly accessible open spaces and parkland throughout the district and along the riverfront so all Clevelanders can experience and enjoy this unique natural resource.
- Accommodate a mix of private and public land uses that provide new opportunities while supporting downtown's historic character and identity.
- Increase development density around GCRTA bus and rail services to maximize the project's transit-oriented development (TOD) potential while knitting together the existing street network and surrounding riverfront context.
- Concentrate density and utilize compact development to promote walkability and create a vibrant and complete neighborhood after each development phase.
- Increased density and height are encouraged, but the design should consider the existing context, buildings, riverfront views, and pedestrians.
- Thoughtfully plan future phases to avoid fragmented development or vacant parcels between sites.
- Address the project area's edges with complementary development in scale, program, and character.

Conceptual Land Use Program



MAP KEY

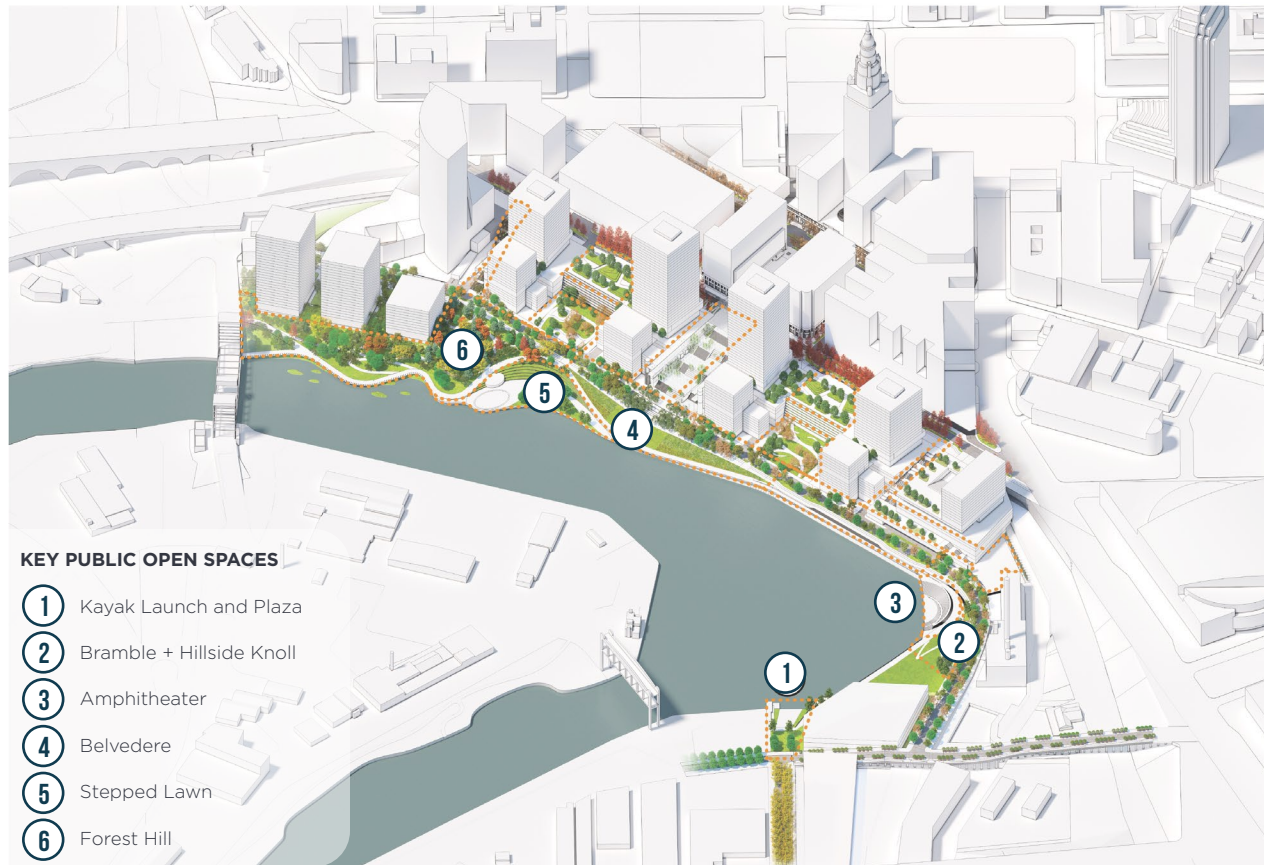
- Potential Public Open Space Parcels
- Potential Private Development Parcels
- Existing Private Development/Buildings to Remain

PROGRAM

Parks + Open Space

Leveraging Public Access and Open Spaces

Parks, plazas, and the landscape are pivotal for transforming the Cuyahoga Riverfront into a vibrant urban hub. The cornerstone is the Riverfront Park and Riverwalk, which ensures equitable public access to the Cuyahoga River. Additional strategically located parks and plazas cater to diverse activities while being designed to reflect the site's unique history and user needs. Each park and open space within the Cuyahoga Riverfront Master Plan project area should enhance outdoor enjoyment and social interaction, offering a diverse palette of experiences.



KEY PUBLIC OPEN SPACES

- 1 Kayak Launch and Plaza
- 2 Bramble + Hillside Knoll
- 3 Amphitheater
- 4 Belvedere
- 5 Stepped Lawn
- 6 Forest Hill

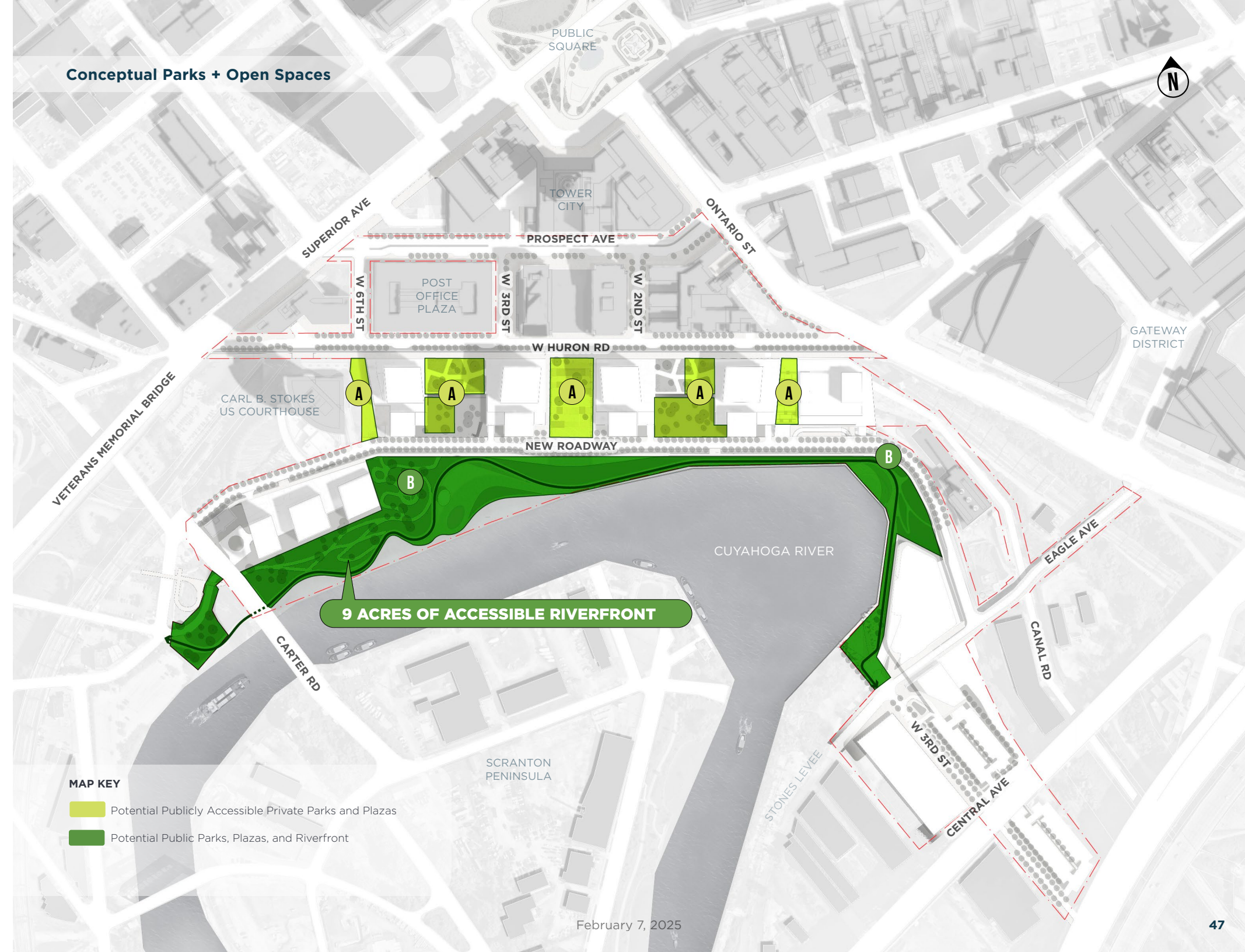
Rendering by Adjaye Associates

Parks + Open Space Objectives:

- Reserve and preserve the edge of the Cuyahoga River as a signature green space that provides waterfront access and activities for all Cleveland residents.
- Leverage the Riverfront Park and Riverwalk as a central spine, providing social, ecological, recreational, and aesthetic functions while adding connectivity and cohesion to all phases and areas of the Cuyahoga Riverfront.
- Anchor the district with a diverse mix of high-quality open spaces that include native plants, ecological functions such as green infrastructure, appeal to the human scale, and promote community and social interaction.
- Support the activation of parks, plazas, and open spaces through adjacent building entries, articulation, activation, and programming.
- Encourage the public to use and visit the neighborhood by making open spaces, including the riverfront, open and accessible from the street, regional bike network, and Tower City Center transit hub.
- Encourage linkages of new riverfront park facilities to surrounding assets such as Canal Basin Park, Tower City Center, Public Square, and the Gateway District.

Open Space Type	Potential Area
A	-3.3 acres
B	-9.1 acres
Total	-12.4 Acres

Conceptual Parks + Open Spaces



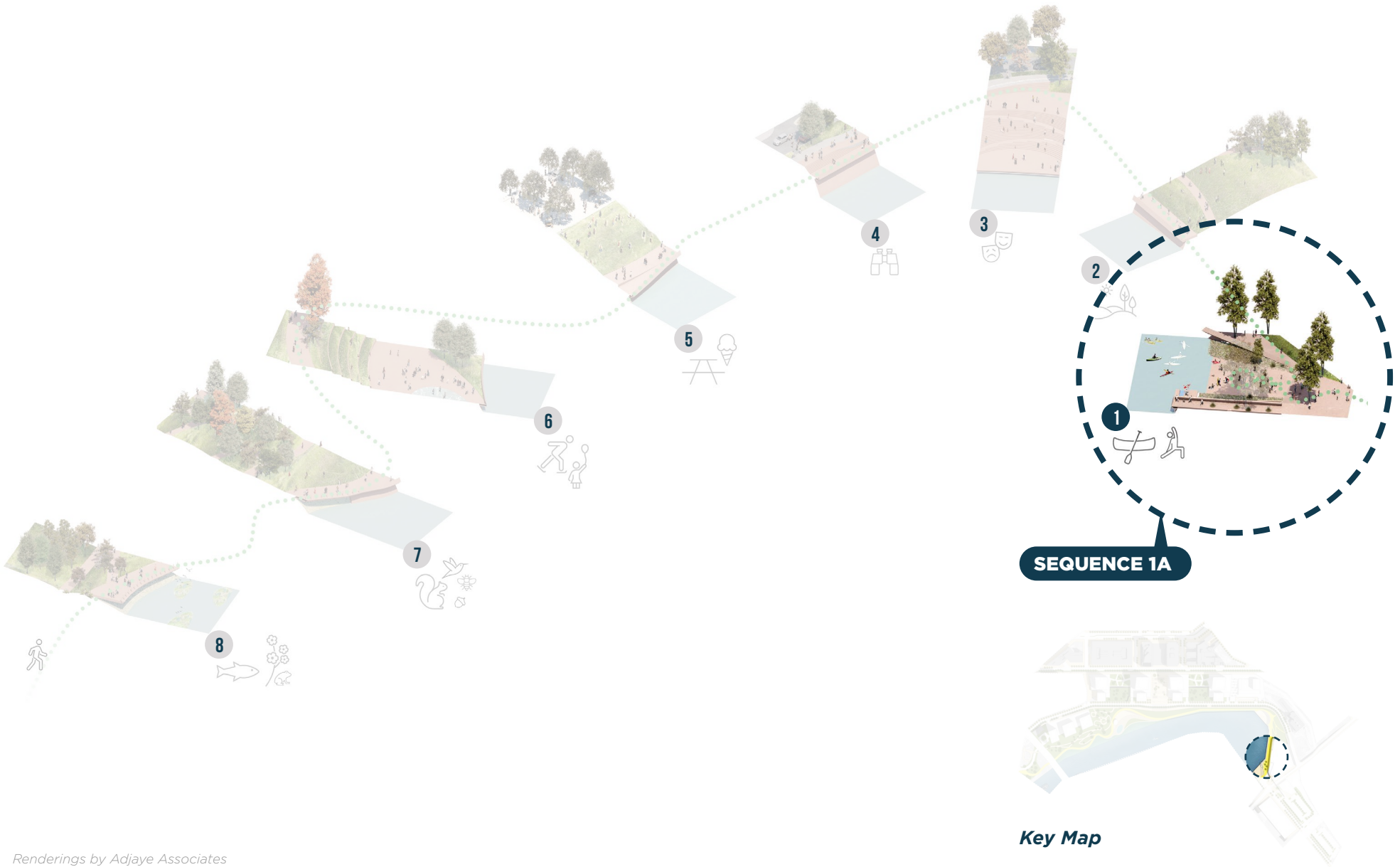
MAP KEY

- Potential Publicly Accessible Private Parks and Plazas
- Potential Public Parks, Plazas, and Riverfront

February 7, 2025

PARKS + OPEN SPACES

Riverfront Park Experiences + Activation



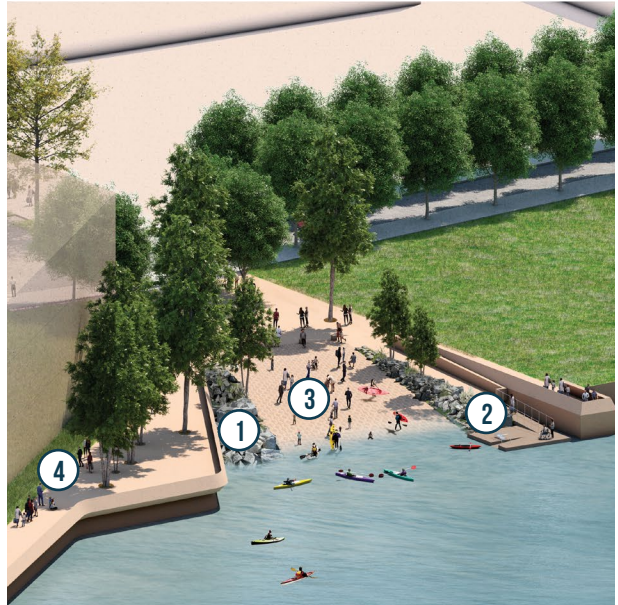
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1. Kayak Launch + Plaza



Riverfront Recreation

Acting as a primary early-stage activation feature, the kayak launch serves as a punctuation point along the Riverwalk and an initial anchor within the overall riverfront public space framework. This feature is positioned to take advantage of an expansive view towards the downtown skyline and the full breadth of the future Cuyahoga Riverfront development, combining river access with a moment of celebration. Primary elements include a generous and safe riverfront launch & accessible floating dock, overlook plaza, and an ecologically sensitive living shoreline design at the eastern edge of the Riverwalk.

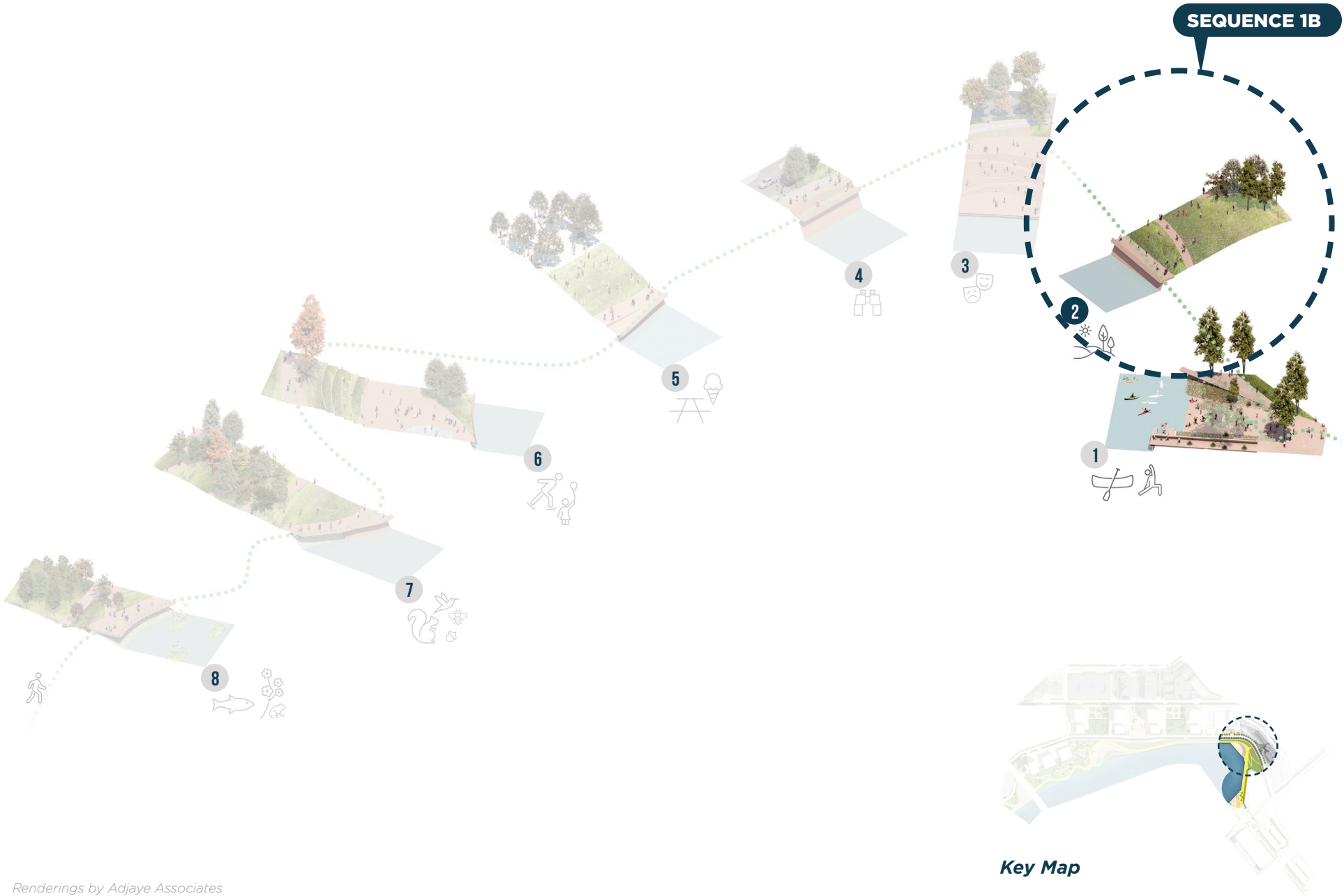


- 1 Living Shoreline + Stone
- 2 ADA Kayak Access Ramp
- 3 Kayak Launch + Plaza
- 4 Riverwalk

Renderings by Adjaye Associates

PARKS + OPEN SPACES

Riverfront Park Experiences + Activation



Renderings by Adjaye Associates

2. Bramble + Hillside Knoll



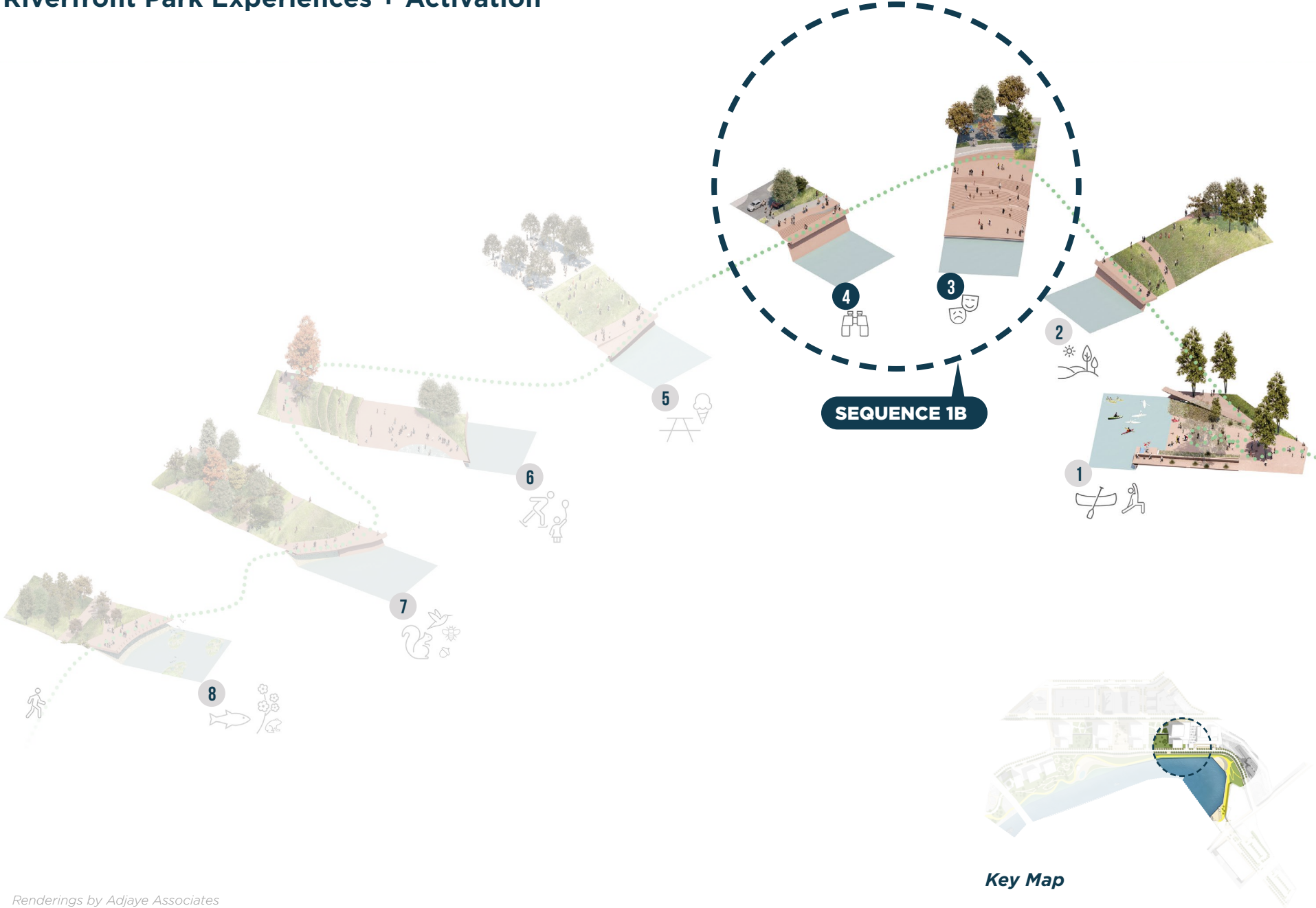
Flexible Open Space

Continuing along the Riverwalk, users will next encounter the Bramble and Hillside Knoll strolling space at Collision Bend where ships over 600 feet long make a sweeping turn on the curving Cuyahoga River. This area features accessible pathways connecting New Roadway down to the Riverwalk, sloping topography for a variety of riverfront vantage points, and flexible space for impromptu seasonal activities.

Renderings by Adjaye Associates

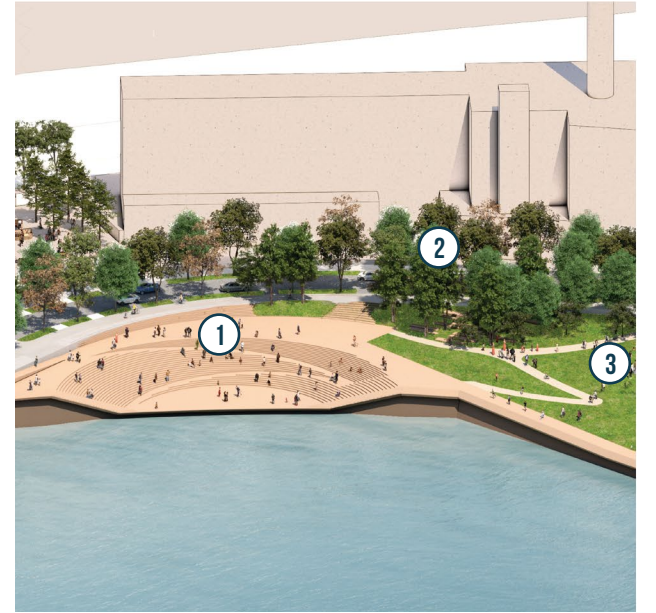
PARKS + OPEN SPACES

Riverfront Park Experiences + Activation



Renderings by Adjaye Associates

3. Amphitheater + 4. Riverwalk



- 1 Amphitheater
- 2 Bramble + Hillside Knoll
- 3 Stormwater Management

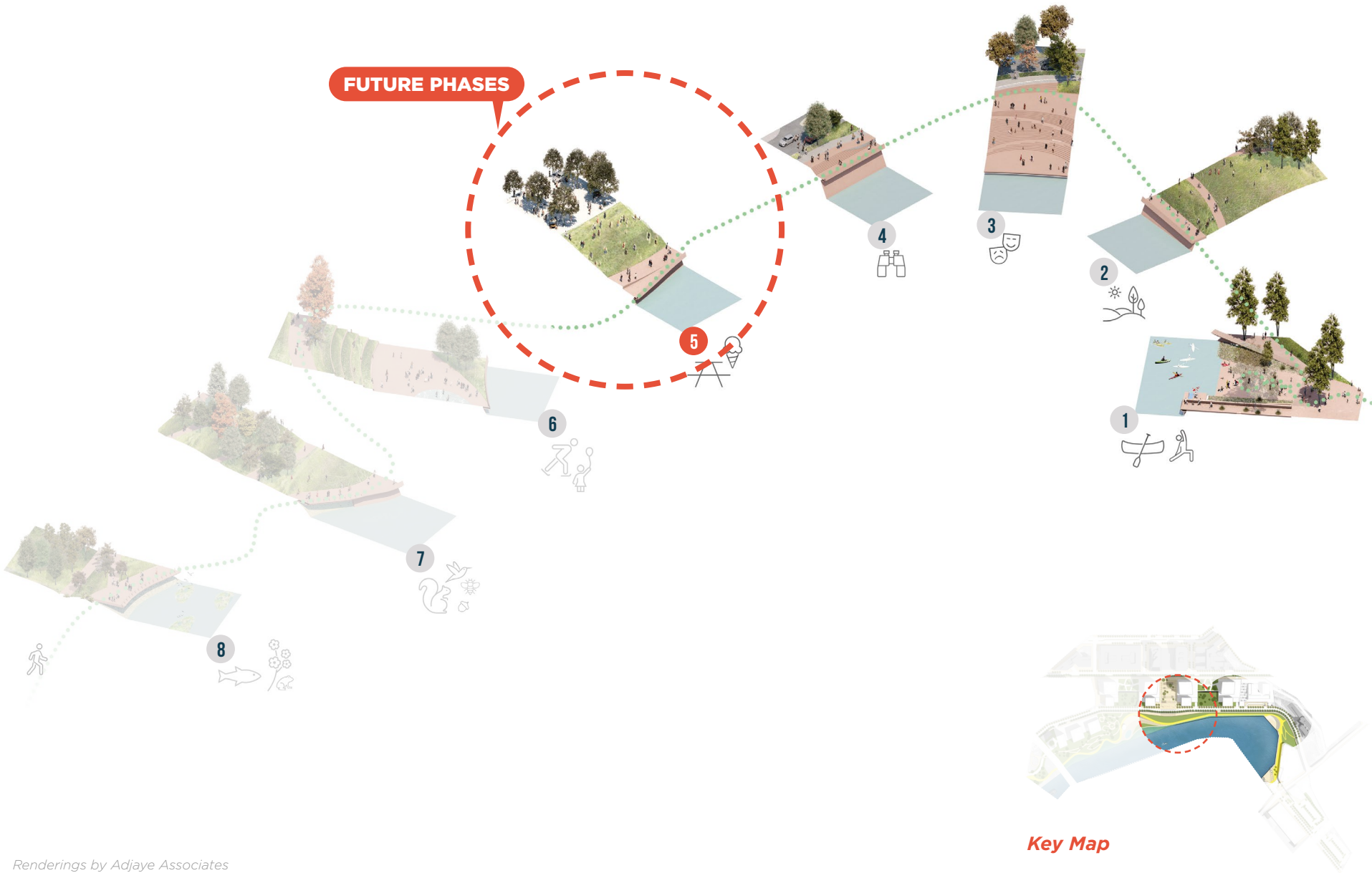
Performance and Elevated Observation

Rounding Collision Bend, the Riverwalk pulls away from the river edge and rises to meet the top of a small amphitheater, positioned intentionally to allow visitors to take in the drama of large carrier vessels maneuvering around the Scranton peninsula, distinct feature and characteristic of this stretch of the winding Cuyahoga River. This area is dedicated to both the passive performance of ship navigation and the potential for active performances that could take place with a fantastic backdrop of the Cuyahoga River shoreline. The Riverwalk itself maintains this higher vantage point as it passes along New Roadway and in front of new building frontages, providing a truly unique and elevated position from which to stroll along the river.

Renderings by Adjaye Associates

PARKS + OPEN SPACES

Riverfront Park Experiences + Activation



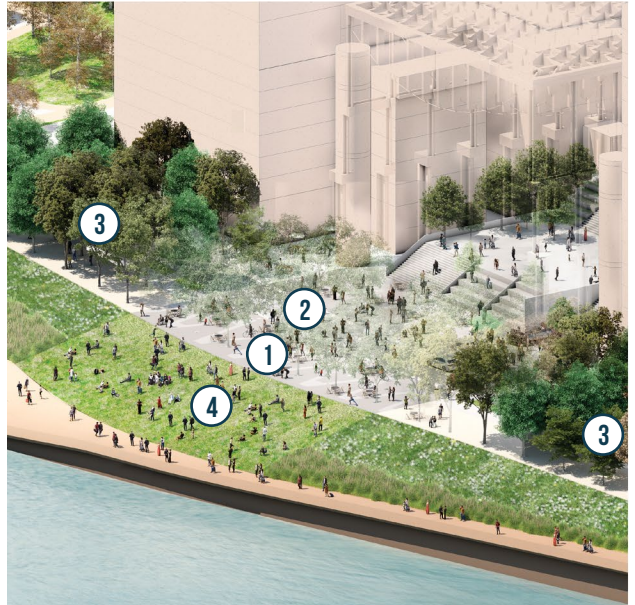
Renderings by Adjaye Associates

5. Belvedere



Core to Shore Connection

Spilling out from the dense urban fabric of Cleveland's urban core, The Porch and The Belvedere provide a direct and generous connection from Public Square, through Tower City Center, and down to the riverfront, opening this previously walled-off connection and inviting users to experience a variety of environments as they descend from the Avenue of Shops to the outdoor plazas, across New Roadway and out to the Belvedere at the riverfront. The sequence is meant to be ever-changing and experienced across a number of pathways and access points to allow for a variety of options and user ability.

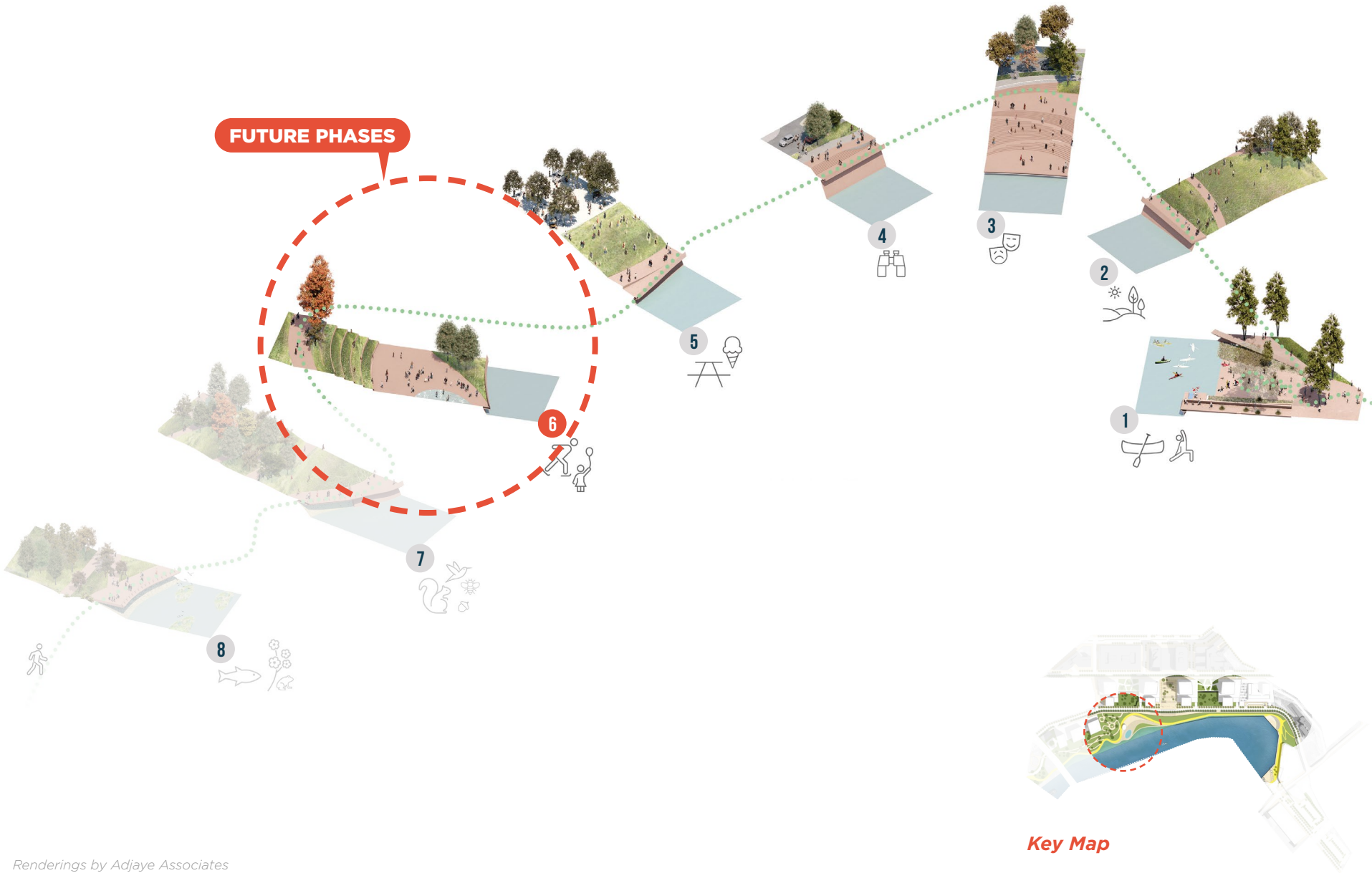


- 1 Belvedere
- 2 Shore to Core to Shore Connection
- 3 Urban Grove
- 4 Sloped Lawn

Renderings by Adjaye Associates

PARKS + OPEN SPACES

Riverfront Park Experiences + Activation



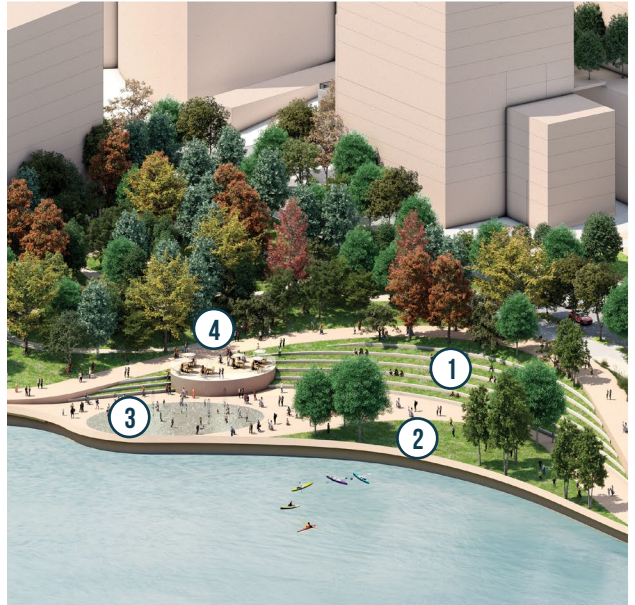
Renderings by Adjaye Associates

6. Stepped Lawn



A Place to Gather

Providing the primary gathering and congregation point along the riverfront, the stepped lawn and activated public plaza offer the largest area for designated programs and community space. Potential programs include a combination splash pad and ice rink facility and associated amenity structure that may include a cafe, rental facilities, park vendors, restrooms, or other community needs. Overlooking this space is a gently rising stepped lawn that provides expansive lounging areas nestled under dappled-shade canopy trees.

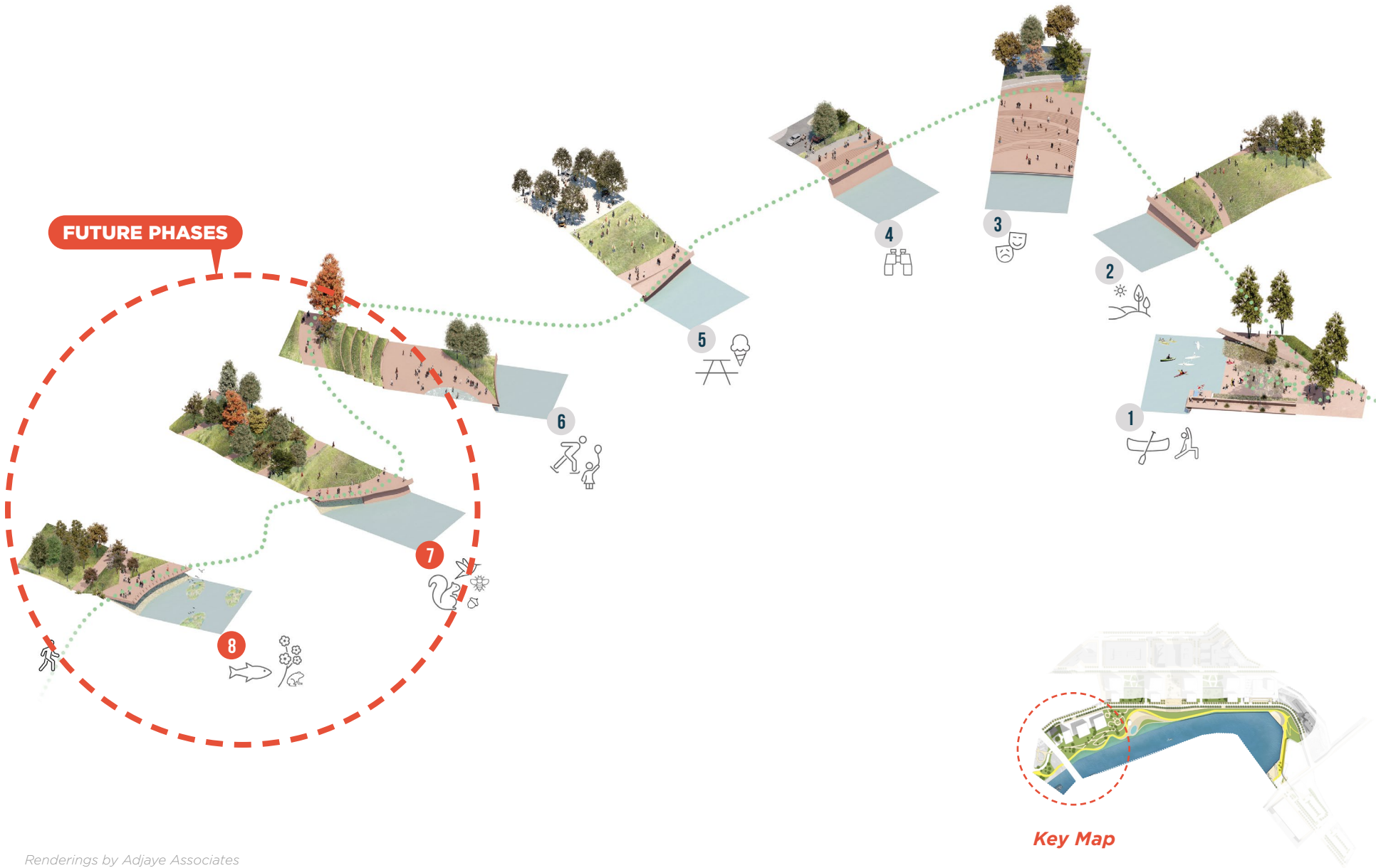


- 1 Stepped Lawn
- 2 Grove
- 3 Splash Pad / Ice Rink
- 4 Kiosk (Cafe / Restrooms)

Renderings by Adjaye Associates

PARKS + OPEN SPACES

Riverfront Park Experiences + Activation



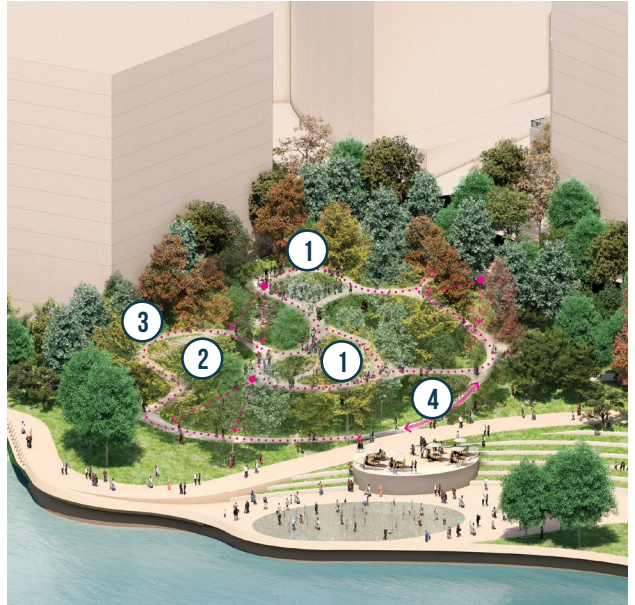
Renderings by Adjaye Associates

7. Forest Hill + 8. Riparian Edge



Expanding the Urban Canopy Cover

Uphill from the riverfront plaza, Forest Hill provides yet another singular experience within the urban fabric and urban forest. Carefully positioned, curated, and established, this experience adds much needed additional shade canopy to an otherwise denuded urban expanse, while allowing for that canopied experience to be visually open, inviting, and maintained to provide ample vantage points both into and out from the woodland strolling area. Accessible pathways meander through this terrain, interrupted occasionally by playful rock scrambles and woodland benches.



- 1 Areas of Respite
- 2 Rock Scrambles
- 3 ADA Accessible Walkway
- 4 Stairs

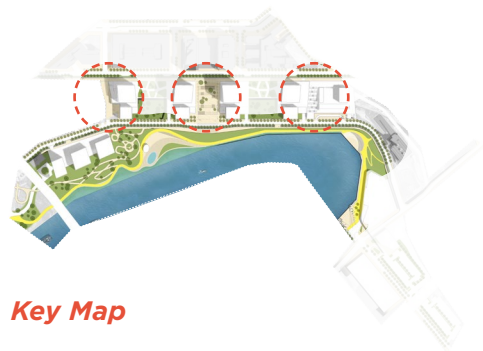
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PARKS + OPEN SPACES

Pedestrian Connections to Riverfront Park Experiences + Activation

Connecting a City back to its River

Critical to the open space framework of the conceptual vision are the connections from the downtown City fabric to the experiences of the Cuyahoga River. The three main avenues for this access, the Cleveland Steps, Courtyard Steps, and The Porch all offer different progressive experiences for connection from the elevated position of Huron Road and Tower City Center down to the riverfront, whether it be an elevated plaza space, a generous front porch and perched plaza connection, or a more direct connection down and through a woodland stroll to the Riverwalk promenade. Each of these experiences is intended to offer a variety of experiences and accessible pathways to allow all Clevelanders to reach the Cuyahoga Riverfront in ways that have not been available in over a hundred years.



TRANSPORTATION + MOBILITY

Vehicular Circulation

Maintaining and Improving Vehicular Access

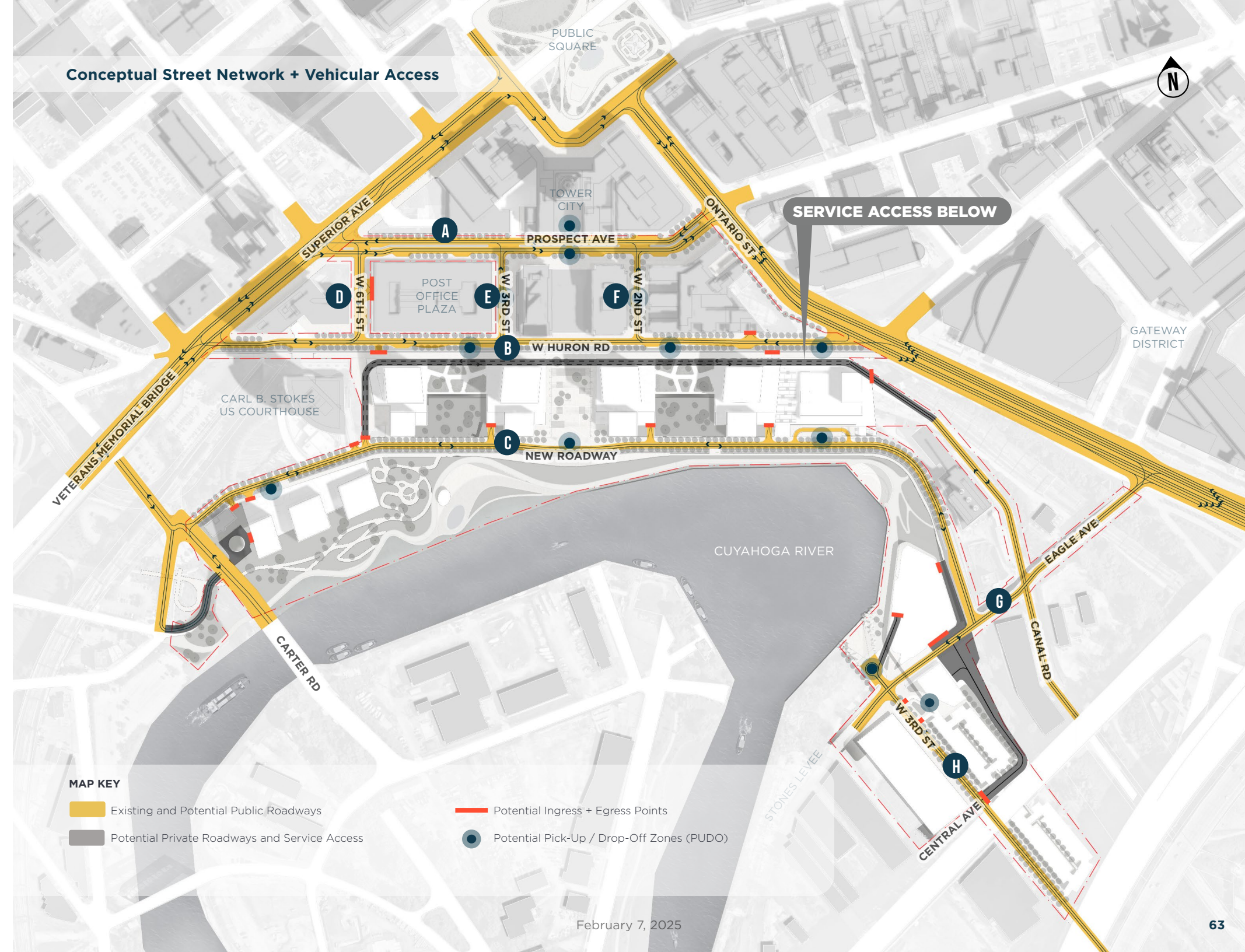
While developing a 15-minute transit-orientated neighborhood is a primary goal of the Cuyahoga Riverfront Master Plan, the riverfront neighborhood must still provide access for all users, including vehicles. Cleveland's existing roadway network serving the Cuyahoga Riverfront Master Plan project area serves two distinct purposes. The upper roadways of Huron Road and Prospect Avenue and the one-block streets of West 6th, West 3rd, and West 2nd that connect them through the project area carry traffic between two major downtown arteries, Superior Avenue and Ontario Street. Prospect Avenue provides efficient transfers between GCRTA bus and rail services for Cuyahoga County. Canal Road is located at the lower level serving the east and west banks of the river including the Flats entertainment districts. This street sometimes carries bulk materials from Cleveland's waterfront docks for use on construction sites and manufacturers through northeast Ohio. The conceptual roadway network and the Master Development Plan's mobility recommendations may be updated to accommodate recommendations from the citywide mobility plan or other future efforts to ensure all proposed infrastructure continues to provide safe access to and from the project area while connecting seamlessly with the regional multimodal network.

Roadway	Lanes	On-Street Parking	PUDO
A Prospect Ave*	4 lanes	No	●
B Huron Rd*	2 lanes	No	●
C New Roadway	2 lanes	No	●
D W 6th St*	3 lanes	No	
E W 3rd St*	3 lanes*	Valet	●
F W 2nd St*	4 lanes	Public	●
G Eagle Ave	2 lanes	No	
H W 3rd St (River Level)	2 lanes	No	

Pick-Up and Drop-Off (PUDO) may include valet parking depending on program
 *Denotes Roadway is currently on structure
 +Denotes One-Way Street

Transportation Objectives:

- Maintain existing vehicular routes while exploring opportunities to implement new connections or variations in the street network's size and purpose. Improvements may include road diets or other streetscape features that support Cleveland's Complete and Green Street Ordinance and subsequent multimodal transportation efforts.
- Integrate multiple travel modes (bicycle, pedestrian, and transit) into the street network throughout the project area to serve all Cleveland residents' current and future needs by providing safe travel options regardless of age, ability, or transportation choice.
- Ensure all vehicular access into a building, parking garage, or development is identifiable and safe through proper urban design elements and signage including limiting the number of curb cuts and possible locations of conflict with pedestrians and cyclists along the roadway.
- Provide service drives for deliveries and other back-of-house access to reduce impacts on public streets and sidewalks.
- Utilize Pick-Up and Drop-Off (PUDO) zones to take advantage of current transportation technologies, limit car dependency, and optimize curb space and efficiency.
- Support Cleveland's ongoing Transportation Demand Management (TDM) Program, use existing infrastructure efficiently and encourage transportation mode shifts at various locations.

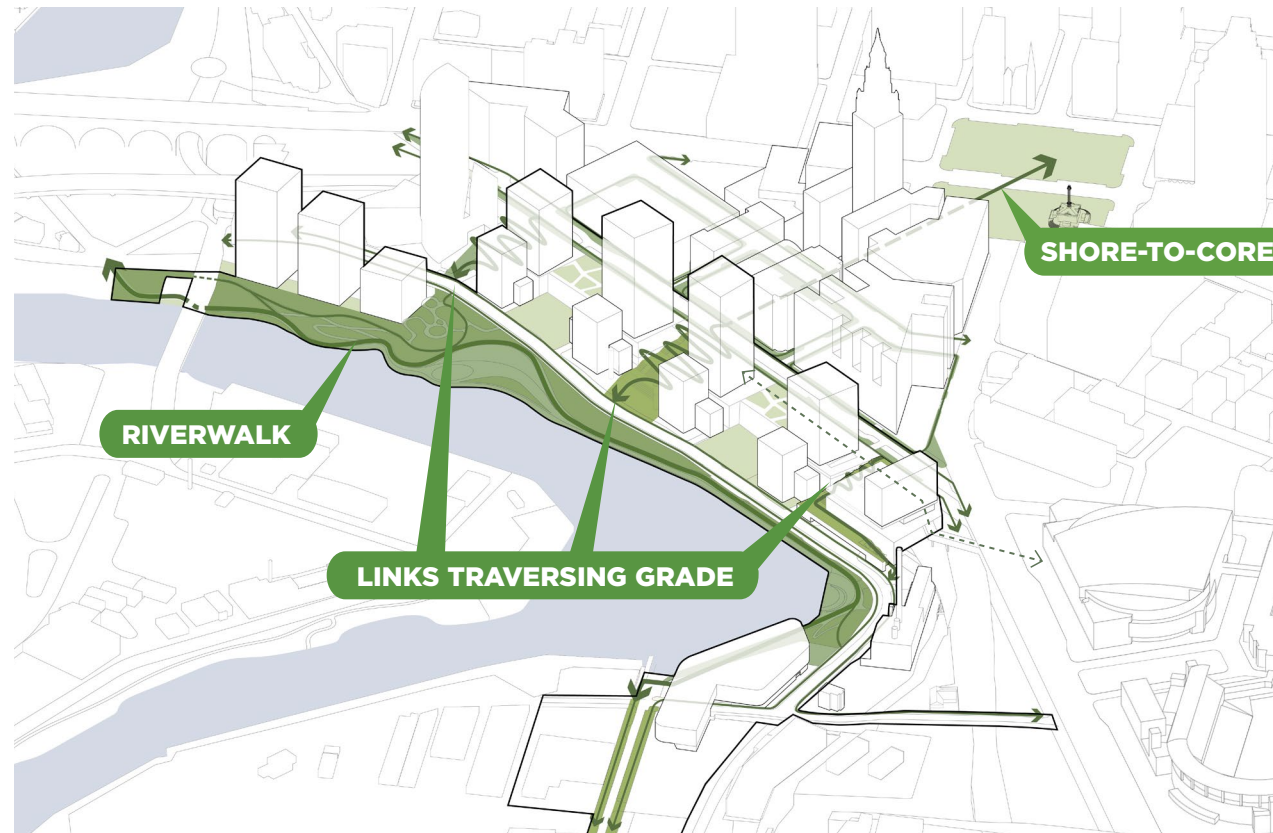


TRANSPORTATION + MOBILITY

Pedestrian Access

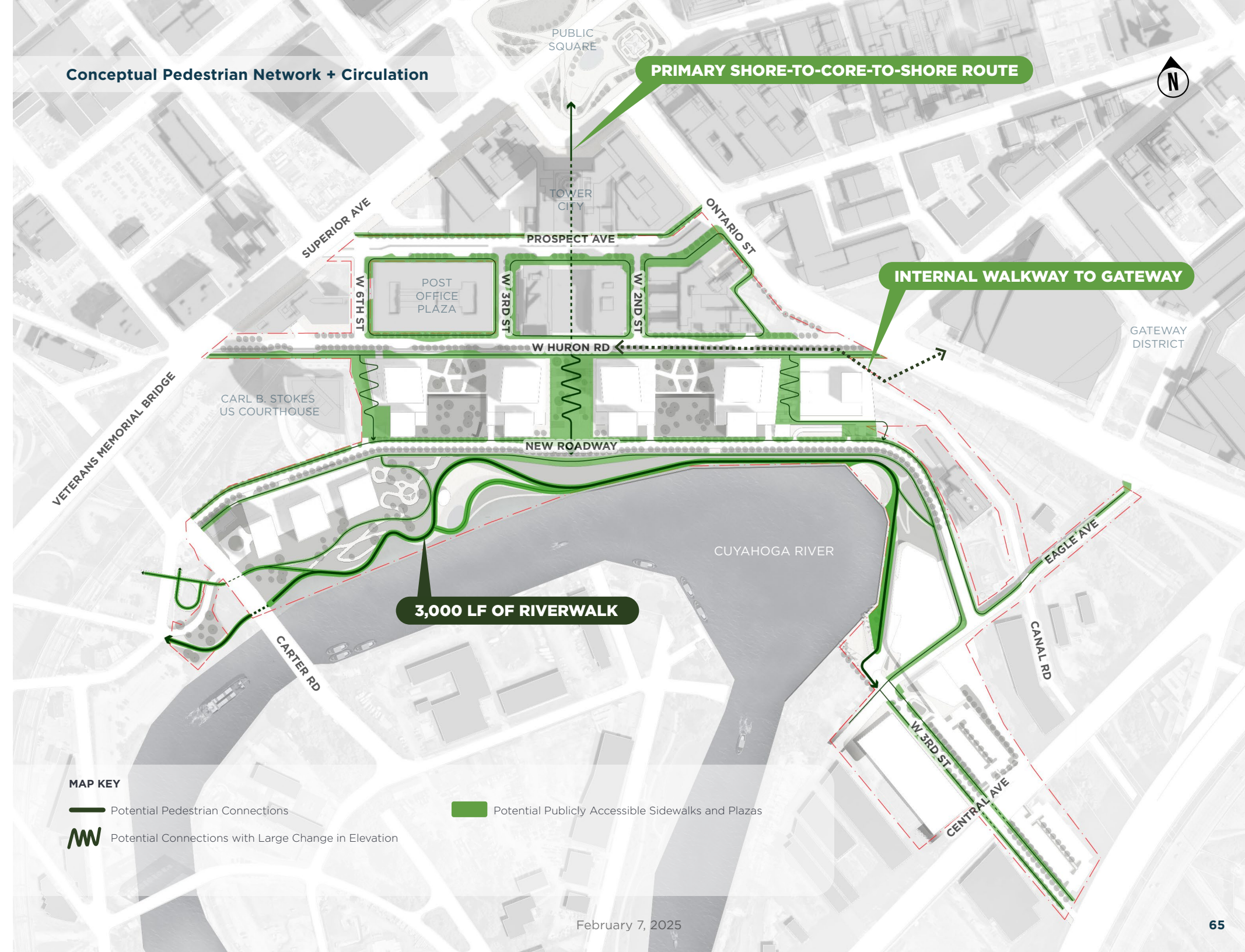
Creating a Walkable Environment

The Cuyahoga Riverfront Master Plan prioritizes pedestrian connections. It creates a walkable environment to support Cleveland's 15-minute city goals, enhance livability, and foster a vibrant mobility network that caters to the needs of current and future residents. This emphasis entails concentrating on walking as a primary mode of travel, thereby minimizing the influence of vehicles on street design, accessibility, mobility, active transportation, streetscapes, and forthcoming development. The objective is to establish an inclusive neighborhood that offers safe, convenient, and accessible pedestrian routes for individuals of all ages and abilities, ensuring everyone walking feels welcomed and accommodated.



Pedestrian Objectives:

- Build a complete pedestrian network with safe street crossings that provide essential access and connections along and across streets and between buildings and civic spaces.
- Ensure all streets have an ample sidewalk zone that allows pedestrians to access open spaces, buildings, and future developments clearly, safely, and comfortably.
- Pedestrian routes should be prioritized and, when possible, physically separated from bicycle facilities to help avoid conflicts between users.
- Develop north-south links through Tower City Center and explore alternative routes that may traverse the steep grade from Huron Road down to New Roadway and the Riverwalk, helping to increase pedestrian access, connectivity, and visibility from Public Square to the Cuyahoga Riverfront.
- Develop a continuous and cohesive Riverwalk that provides a critical link and amenity along the river for the district and the City of Cleveland.
- Design the Riverwalk in a way that balances the spirit and intent of the Flats Design Guidelines while creating a unique identity and character for the neighborhood and this segment of the Cuyahoga Riverfront.
- Celebrate the views of the downtown skyline, Cleveland's unique bridges, and activity on the Cuyahoga River throughout the project area.
- Connect the riverwalk with the regional trail network extending throughout greater Cleveland.
- Leverage adjacencies and connections to Tower City Center to help enhance GCRTA Bus and Rail access.



MAP KEY

— Potential Pedestrian Connections

— Potential Connections with Large Change in Elevation

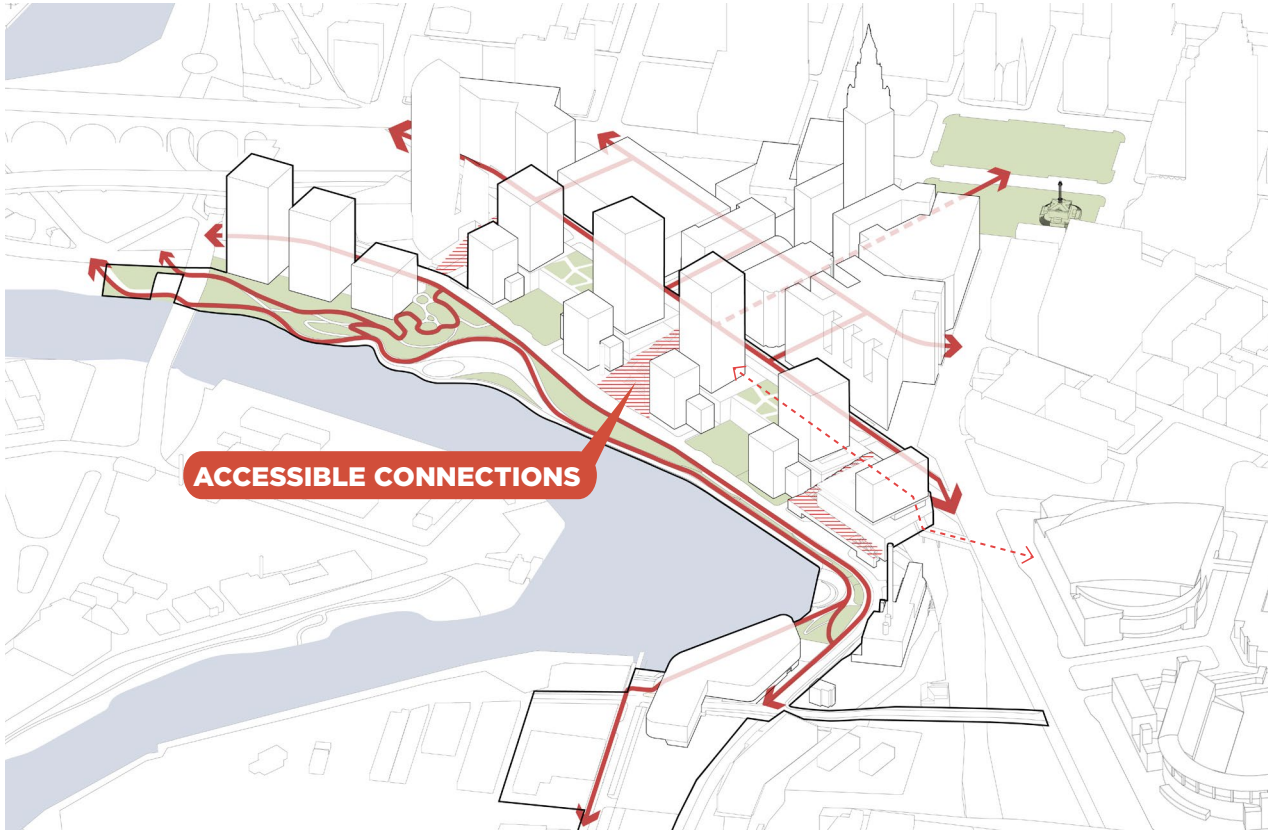
■ Potential Publicly Accessible Sidewalks and Plazas

TRANSPORTATION + MOBILITY

Accessibility for All

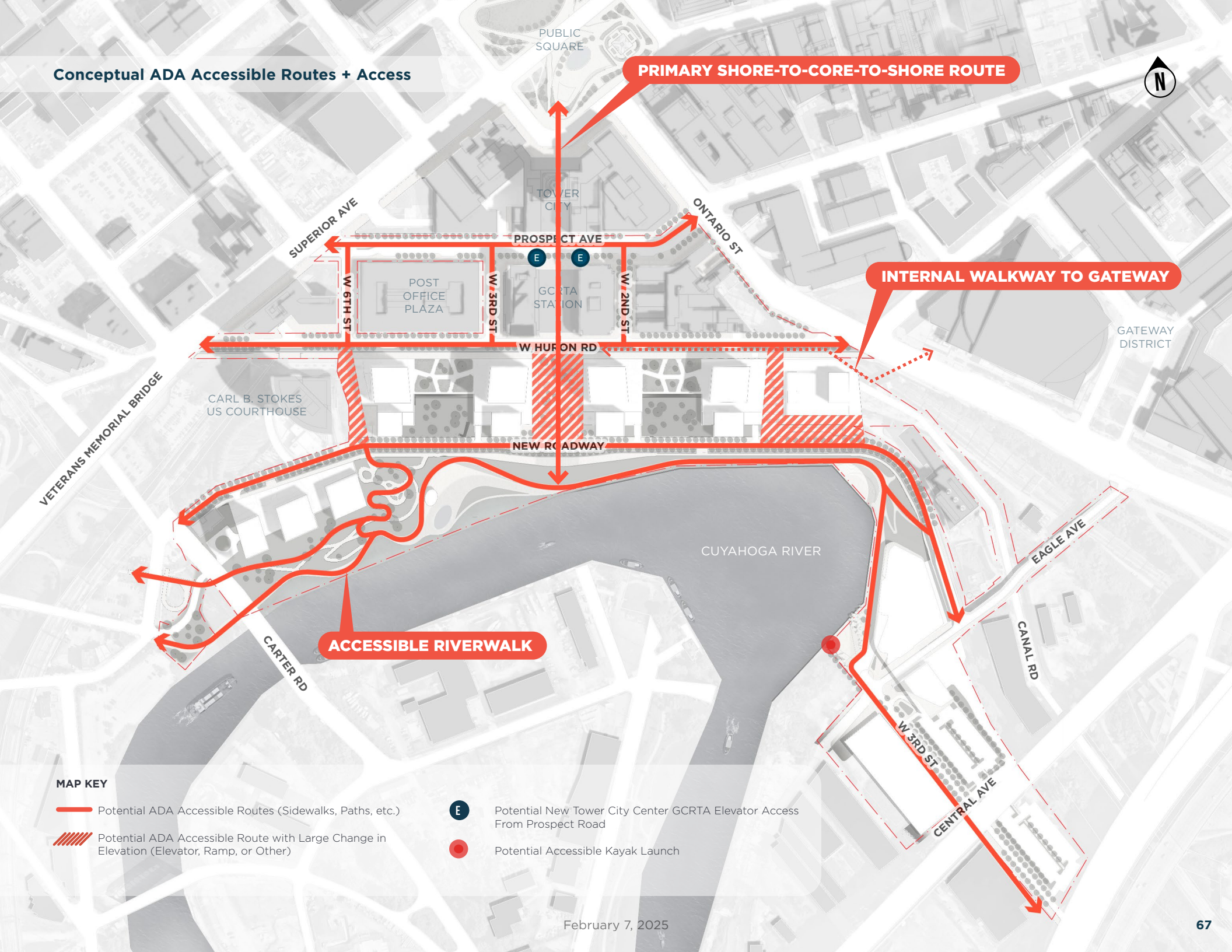
All Ages + Abilities

Equitable access is a critical component of the Cuyahoga Riverfront Master Plan. The district's vision emphasizes intuitive and contiguous sidewalks, pathways, trails, and crossings that accommodate all users. The slope challenges, particularly those between Huron Road and the riverfront, outline the need for a wide range of strategic, creative, and context-sensitive access solutions, from elevators to ramps and other features that help address the grade changes within the project. Leveraging Tower City Center's existing infrastructure, including the addition of elevators and other future connections, help support Cleveland's Shore-to-Core-to-Shore efforts with a direct and accessible route from Public Square to the Cuyahoga River.



Accessibility Objectives:

- Utilize the proximity of GCRTA's Bus and Rail Hub at Tower City Center as a convenient route to the Riverfront.
- Provide public open spaces and amenities that are accessible to all-abilities and generations.
- Where feasible, provide all-abilities access along all major street connections and the Riverwalk.
- Where changes in vertical elevation are severe, provide all-abilities access through a combination of elevators, ramps, and sloped walkways or other creative solutions that provide for a similar level of effort and route to other options available.
- Provide smooth transitions, routes, and vertical circulation for all-abilities and generations to access amenities and public transit.



MAP KEY

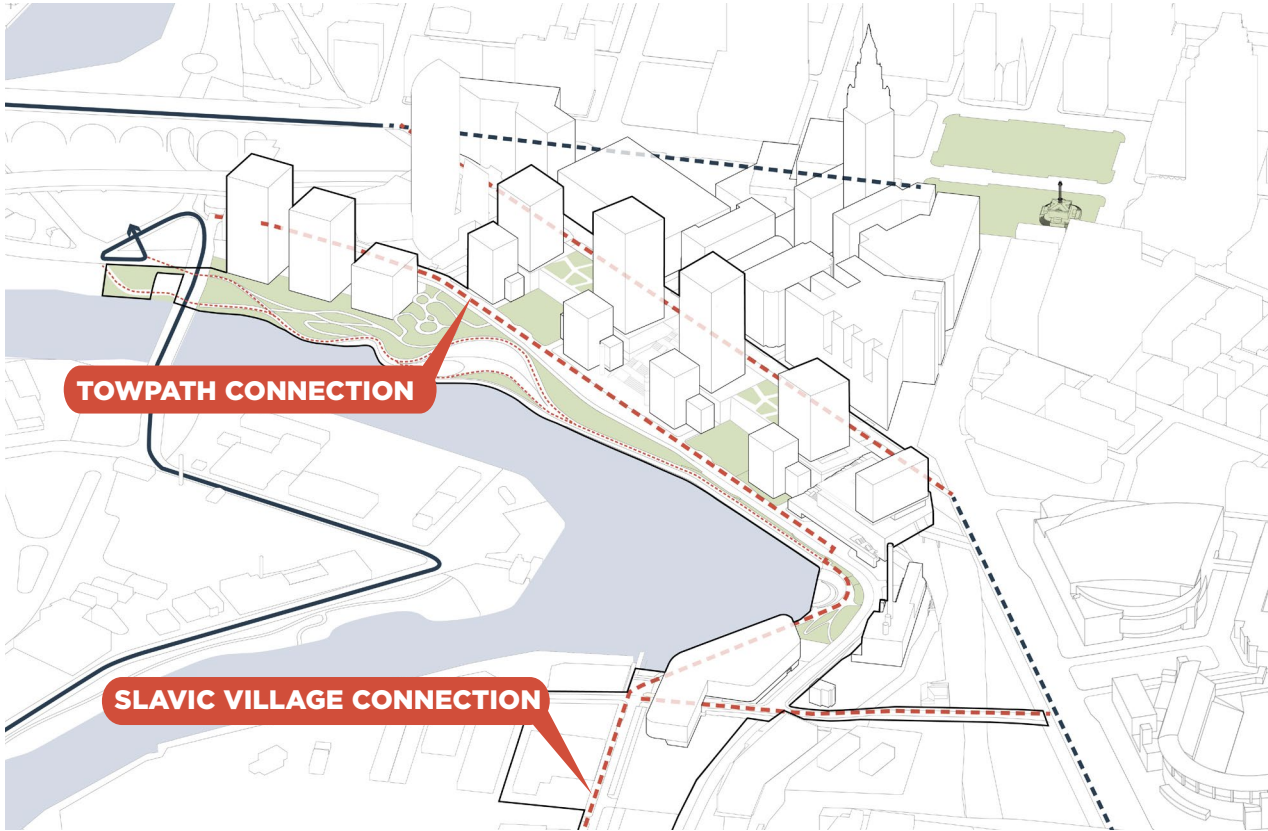
- Potential ADA Accessible Routes (Sidewalks, Paths, etc.)
- ▨ Potential ADA Accessible Route with Large Change in Elevation (Elevator, Ramp, or Other)
- E Potential New Tower City Center GCRTA Elevator Access From Prospect Road
- Potential Accessible Kayak Launch

TRANSPORTATION + MOBILITY

Bicycle Access

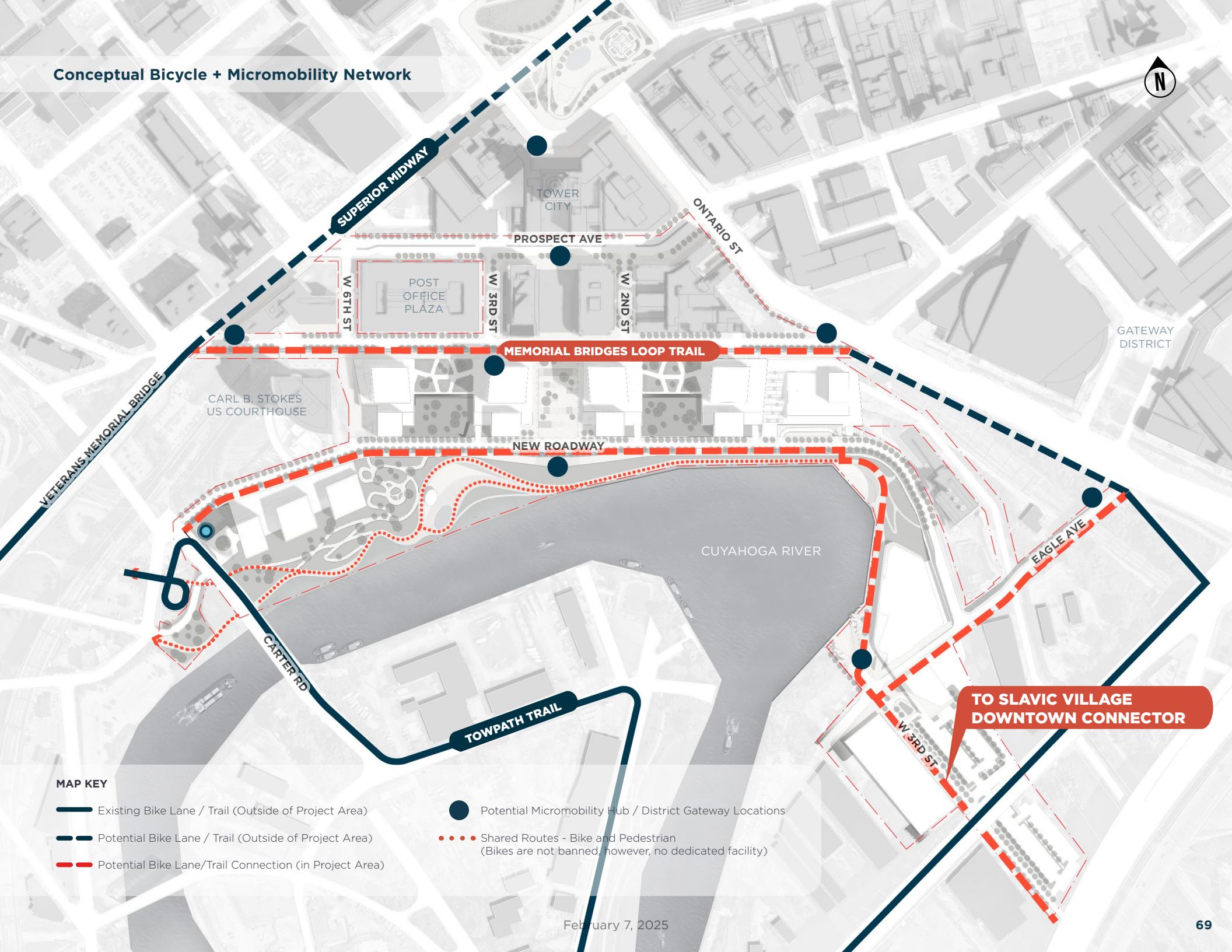
Active Transportation Network and Micromobility Options

Greater Cleveland has a growing network of bicycle facilities, many extending from the 100-mile Towpath Trail along the Cuyahoga River. New bicycle infrastructure should build upon these existing routes and provide linkages to parks and development in the project area, downtown Cleveland, and the City's neighborhoods. Expanding and enhancing bicycle infrastructure means ensuring a robust network to make bicycling a more viable mode of travel for residents and visitors. It also means providing a safe and comfortable infrastructure for all users. All new bicycle facilities must work to capture more riders, increase mode shift, and support Cleveland's infrastructure and mobility network goals.



Bicycle and Micromobility Objectives:

- Design roadways and intersections throughout the district to promote biking and the use of micromobility options, which can reduce dependence on vehicles and create a safe environment for all users.
- Ensure that the edges of the project area accommodate any existing bike facilities surrounding the district.
- Incorporate recommendations from Cleveland Moves: the Citywide Mobility Plan, Transportation Demand Management (TDM) Program, and other local and regional networks into the district's recommended bike facility network.
- Create new routes connecting to existing and potential new trail infrastructure outside the project area, including the existing Towpath Trail, the Cleveland Midway, and the Memorial Bridges Loop Trail.
- Provide amenities (bike repair, bike parking, bike share, and others) along the streetscape and at critical locations in the district, such as strategically in or near publicly accessible open spaces and near transit stops, that serve bicyclists and micromobility users.
- Leverage the district's location and its potential to fill gaps in Cleveland's bicycle network.
- Employ bike and micromobility hubs that accommodate users' needs, allow for mode change, and make using all modes easier and more convenient.



MAP KEY

- Existing Bike Lane / Trail (Outside of Project Area)
- - - Potential Bike Lane / Trail (Outside of Project Area)
- - - Potential Bike Lane/Trail Connection (in Project Area)
- Potential Micromobility Hub / District Gateway Locations
- Shared Routes - Bike and Pedestrian (Bikes are not banned, however, no dedicated facility)

TRANSPORTATION + MOBILITY

Public Transit Access

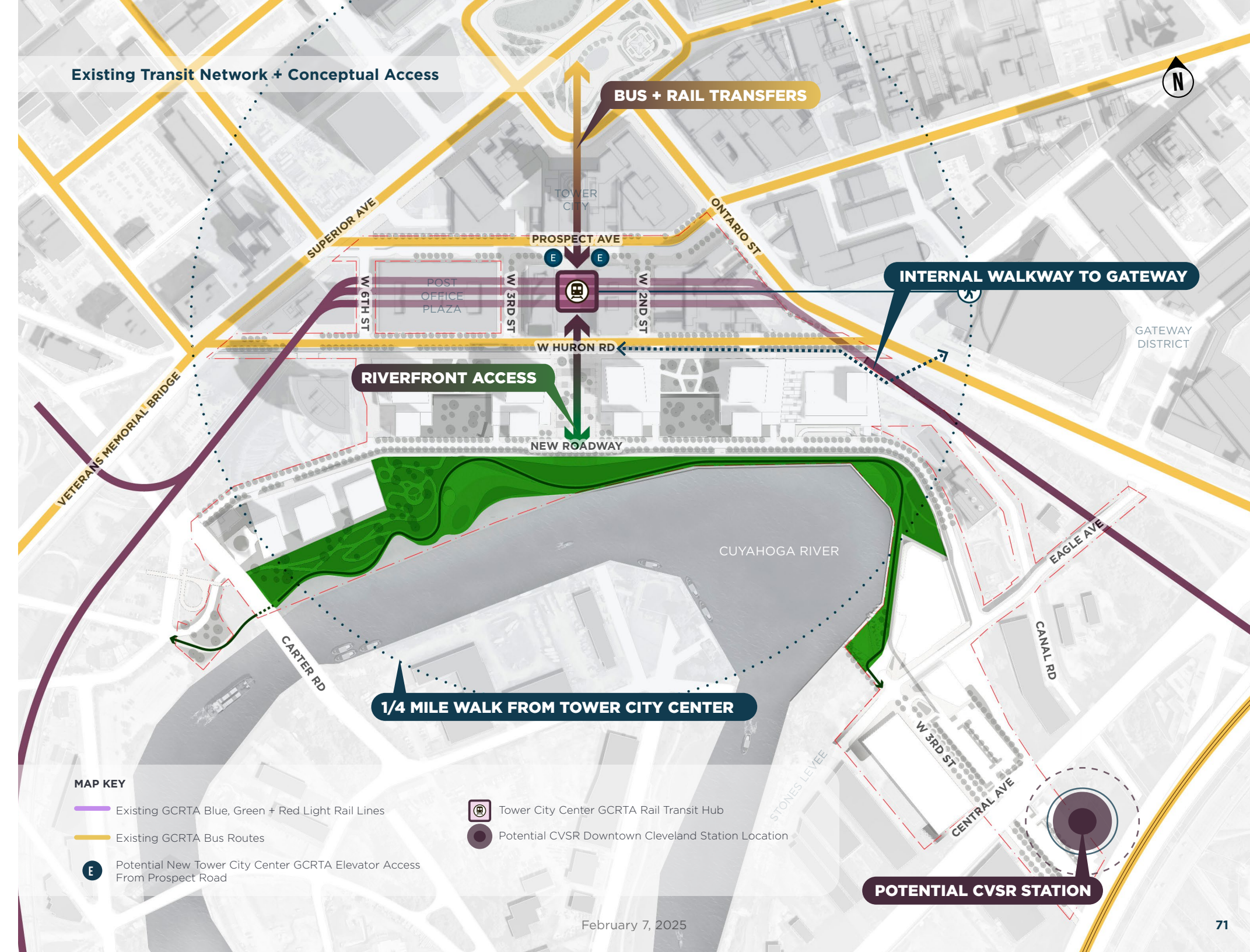
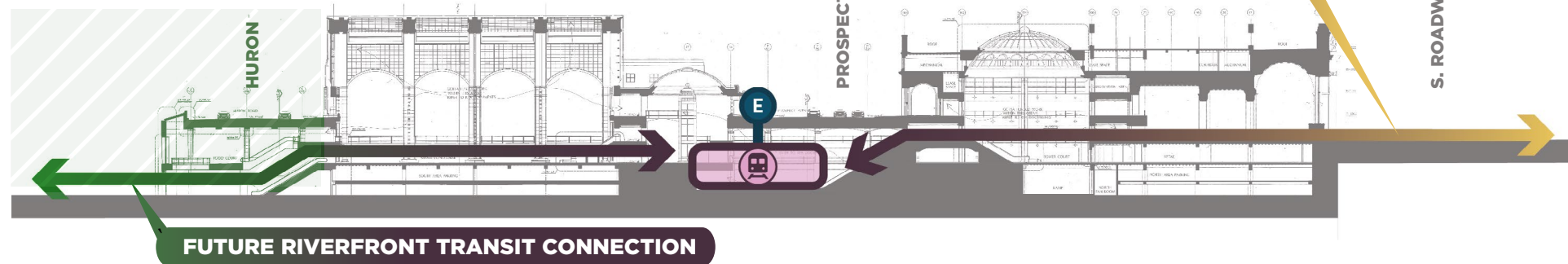
Transit Orientated Neighborhood & Riverfront

The Cuyahoga Riverfront Master Plan embodies an accessible neighborhood with easy access to GCRTA bus, trolley, Bus Rapid Transit, and rail services. Nearly all of the district is within a quarter mile of Tower City Center and Public Square transit hubs, creating one of downtown Cleveland's largest transit-orientated development opportunities. Tower City Center itself is a 'city within a city' with hotels, retail office, and residential linked by interior walkways with direct access to a GCRTA rail station serving all lines. The district's land use development program, residential density, and building placement should all support transit ridership, providing convenient and desirable access to transit stations and stops within the project area.

Transit Objectives:

- Enhance internal and external access and connections to GCRTA service at Public Square and Tower City Center to support operations, existing properties, and future development.
- Identify opportunities to enhance connectivity between Tower City Center Rail Station and the project area, such as potential elevators from Prospect Avenue and accessible walkways to the riverfront.
- Ensure all streetscapes provide the space and the amenities necessary to support accessible and comfortable transit-waiting environments.
- Identify opportunities to repair, rebuild, or relocate, the GCRTA walkway from Tower City Center to Rocket Mortgage FieldHouse.
- New bus stops and enhanced internal connectivity should be added to increase mode shift opportunities and transit access from all locations.
- Ensure roadway dimensions and streetscape designs have adequate space for the operation of transit movements where needed.
- Explore opportunities to directly link GCRTA's Settlers Landing station with development along New Roadway and the proposed Riverfront Park.
- Explore opportunities and design ideas to help integrate a new Cuyahoga Valley Scenic Railroad station, extending its service from the Cuyahoga Valley National Park to downtown Cleveland.

Tower City Center Riverfront and Transit Connections

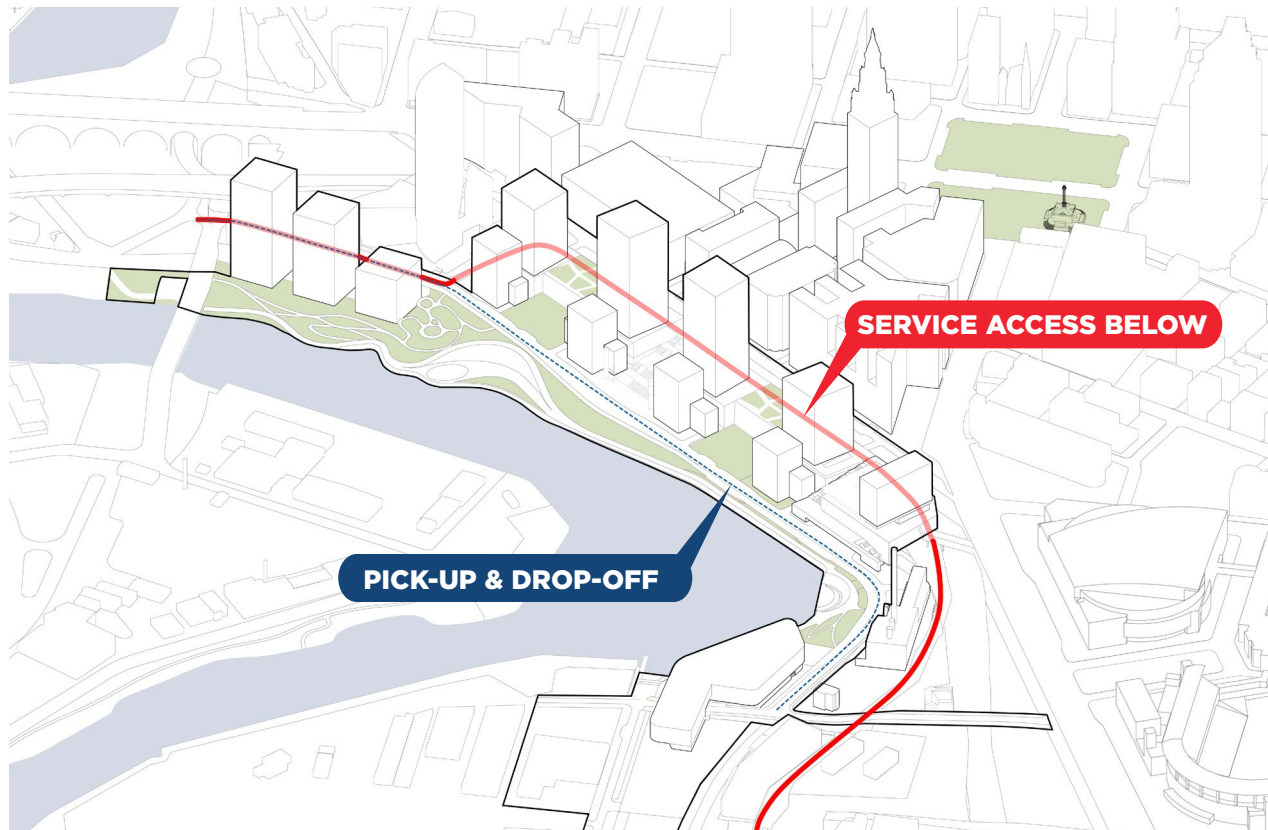


TRANSPORTATION + MOBILITY

Service Access

Prioritizing the Public Realm

Service vehicles will use Canal Road on the east and the proposed below-grade private service road extension, before exiting onto New Roadway on the west by the U.S. Courthouse. This internal drive will service much of the bustling Tower City Center and new development between New Roadway and Huron Road. This is expected to reduce truck traffic on New Roadway. Staging or queuing of service and delivery vehicles is not permitted on the perimeter of public streets unless a dedicated delivery lay-by zone is provided. The location of service components, utility infrastructure, and loading facilities should be carefully designed to prioritize the public realm experience while providing effective access. Whenever feasible, all utilities (stormwater, wastewater, electrical, gas, etc.) shall be located underground, preferably in roadways.



Service Objectives:

- Service access should be efficient, but service activities should remain out of view and not conflict with public spaces, pedestrian access, or views.
- Multiple facilities are encouraged to share service and loading areas to the greatest extent possible.
- Service entrances and ramps are prohibited within or fronting major public open spaces.

PUBLIC ART

Interest + Identity

Enriching, Enhancing, and Engaging

Public Art is a critical component of the built environment and adds much to the city's character, history, aesthetics, and quality of life. It is permitted throughout the City of Cleveland and should be encouraged and featured throughout the project area. Public art projects should include pop-ups, temporary experiences, spatial artwork (sculpture), and murals. These pieces should be carefully curated to provide interactive and unique methods for storytelling and education on the riverfront and the site's unique place in Cleveland's past and future.



Public Art Objectives:

- Encourage the integration of public art in the district's buildings and public realm so they are not placed as isolated objects or utilized to overcome design shortcomings.
- The community should be engaged in the development of permanent art installations and local artists should be considered along with international artists.
- Engage with artists earlier in the planning and design phases to avoid artwork feeling placed as an afterthought.
- The design and selection of art pieces should be influenced by the site's physical character, as well as its cultural history.
- Art should be designed and installed so as not to damage or obscure contributing features such as buildings or river views or impact adjacent properties with light or sound that emanates from them.
- Artistic lighting for screening or as a dramatic feature can help honor Cleveland's rich history of innovation and industry.
- Artworks should be crafted and installed with maintenance and durability in mind.
- Temporary art installations should be explored to activate spaces for specific seasons, events, or programs.
- Public art installations should be coordinated with the City of Cleveland's Senior Arts Strategist to ensure compatibility with other efforts and standards.
- Art locations, materials, mediums, and other criteria such as quality, appropriateness, safety, feasibility, and maintenance will be evaluated and approved per City of Cleveland requirements.

PUBLIC SIGNAGE

Navigation + Information + Identification

Develop a Unique Brand and Identity

The Cuyahoga Riverfront Master Plan will transform the project area with new developments, buildings, parks, and plazas. Therefore, when a critical mass of development and infrastructure is complete, it may be beneficial to develop a signage system that represents the character and identity of Cleveland and the project. A potential future signage master plan presents an opportunity to tell the district's story, link key destinations, and create community pride. Accurate, reliable, and easy-to-understand signs are important for any neighborhood, especially those where pedestrian and bicycle use is a priority. A signage system with a distinct brand and style provides easy-to-follow and legible directions and can give visitors an experience unique to the Cuyahoga Riverfront.

Any future signage master planning study will help establish detailed design and location. Once completed, the MDP may be updated with a future signage master plan and branding study. The primary objective of a wayfinding and signage design guide is to promote and strengthen the sense of place, support the neighborhood brand, and celebrate what is unique about the project area. As future signage and branding standards emerge, the signage family may be unique but also complementary to the existing Destination Cleveland brand and sign family.

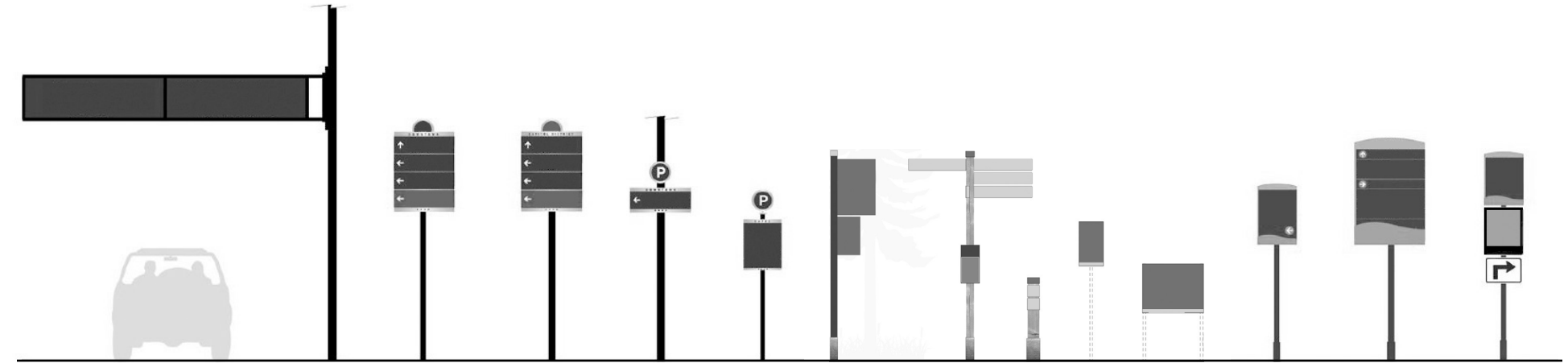
Public Signage

Branding, signage, and wayfinding materials reinforce neighborhood boundaries and provide information that allows travelers to navigate the area without local knowledge, mobile phones, or maps. The information communicated through signage increases visitors' comfort level and conveys a positive experience while traveling to one's destination, increasing accessibility and confidence for travelers to improve the users' experience. It boosts the opportunity for return visits and leaves guests with a more favorable view of the Cuyahoga Riverfront. The location, look, or style of the sign may vary; the primary use of wayfinding signage typically falls into one of the following categories.

- **WAYFINDING AND DIRECTIONAL SIGNAGE** - Directional signage is a visual communication tool that typically points the driver, visitor, or observer toward one or more destinations.
- **INFORMATIONAL SIGNAGE** - Maps, kiosks, and diagrams are all good examples of informational signage.
- **REGULATORY SIGNAGE** - These signs focus on safety, liability, and compliance with regulations and standards, featuring clear and accessible messaging. Examples include hours of operation, access restrictions, and other safety considerations.
- **IDENTIFICATION SIGNAGE** - Identification signs are one of the most common types of wayfinding signs; they can encompass a broad spectrum of uses, from gateway signage or other simple labels and markers at important or unique destinations.

Public Signage Master Plan Objectives:

- Bolster the district brand and identity.
- All signage should take into consideration any city-wide signage or other wayfinding/activation initiatives.
- Complement or utilize the existing Destination Cleveland wayfinding and signage system to maintain uniformity without limiting or detracting from district specific opportunities.
- Identify gateways and key entry points.
- Convey clear vehicular wayfinding to parking and building facilities.
- Convey clear and safe wayfinding to parks and buildings for all modes of travel including transit vehicles, pedestrian, bicycle, and watercrafts (kayak, paddle, canoe)
- Allow for proper introduction and regulation of technology including touchscreen, video boards, digital signs, and marquees.
- Improve visual aesthetics, reduce visual clutter, and prevent the overuse of signage.
- Identify opportunities for storytelling or other markers about the site and city's history.
- Expand the wayfinding system to include Tower City Center and the GCRTA station, creating a seamless experience for transit users.



Wayfinding and Directional Signage Examples (pedestrian and vehicular)



Informational Signage Examples

Identification Signage Examples

CONCEPTUAL STREETSCAPES

Zones of the Street

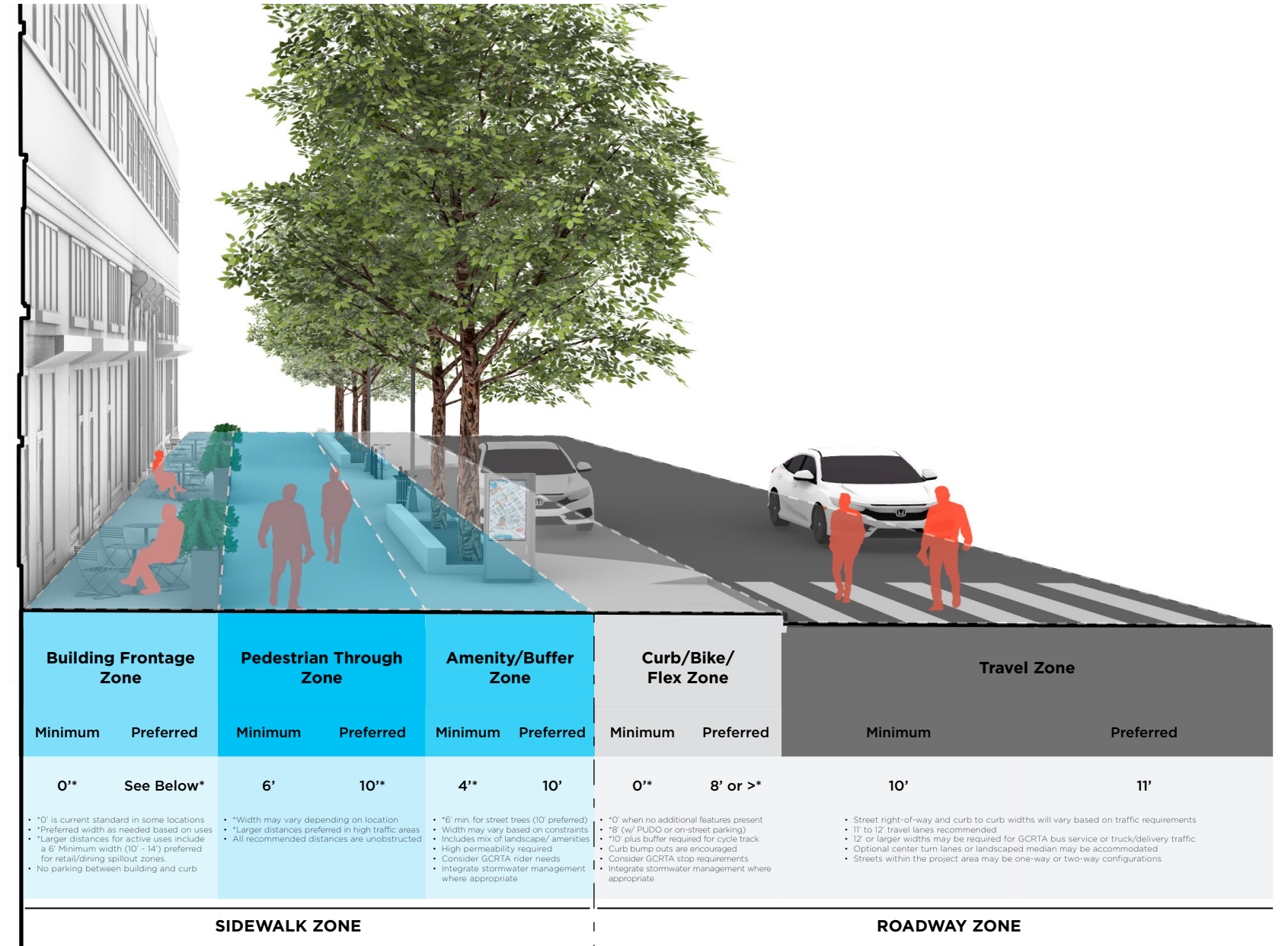
Creating Pedestrian-Scaled Environments

A street includes the entire area within the public right-of-way but may also have privately owned public space (POPS), which is land that is open to and made available to the public. Various functions may occur within this public realm, requiring street designs that accommodate more than just car traffic. For the purpose of the Master Development Plan, roadways and public realm areas are broken down into zones with distinct functions and design elements to support those functions. Combining all these zones is what defines a street's quality and character. Street right-of-way is valuable public space, and when appropriately designed, a road can be an inviting and welcoming environment that provides the amenities needed for cars, pedestrians, bicycles, and transit. To achieve great streets, it is imperative to design each zone correctly.

- **TRAVEL ZONE** - The travel zone is dedicated to the through movement of vehicular traffic. Generally auto-oriented, it accommodates transit systems and may include on-street bike facilities such as sharrows or other types of facilities. In the Cuyahoga Riverfront Master Plan, the roadway zone will vary from two travel lanes to multiple lanes, including the possibility of turn lanes, medians, or one-way streets.
- **CURB / BIKE / FLEX ZONE** - This area is typically used for parallel, on-street parking, pick up/drop off areas (PUDO's), and potentially curb adjacent bike facilities such as bike lanes (buffered or protected) and cycle tracks on one or possibly both sides of the street. For suggested bike facility typologies, design treatments, and locations please refer to the conceptual streetscapes on the following pages. The Curb/Bike/Flex Zone is also where the pedestrian space may be extended into the parking or drop-off lane for features such as transit stops, bulb-outs, and mid-block curb extensions.
- **AMENITY/BUFFER ZONE** - This area, generally within the public right-of-way, is adjacent to the sidewalk and is home to street trees, landscaping, streetlights, traffic control, wayfinding signs, and site furnishings. Also referred to as the buffer zone, this area provides necessary physical separation between pedestrians and vehicular travel. The amenity zone may also include the site, generally referred to as an edge zone, which is the back of the curb area used by people getting in and out of vehicles curbside. Maintaining this zone's dimensions, material, landscaping, and site furniture standards helps establish consistency and quality in the pedestrian experience.
- **PEDESTRIAN THROUGH ZONE** - This portion of the street is dedicated to the unobstructed movement of pedestrians, and in some cases, it can be expanded to include a separate off-street bike facility via a multi-use path or cycle track at sidewalk level (i.e. The Memorial Bridges Loop Trail). The pedestrian through zone is subject to specific standards to comply with the Americans with Disabilities Act (ADA). Establishing broad sidewalk standards with high-quality materials will help reinforce the desire to prioritize accessibility and pedestrian travel throughout the project area.
- **BUILDING FRONTAGE ZONE** - This portion of the street is adjacent to the property line. It includes transitioning from the public sidewalk to those quasi-public-private areas beyond the public right-of-way that may include building entrances, forecourts, parks, plazas, and outdoor dining areas. This zone may provide a transition from sidewalk to building and public to private, while creating additional space to help activate the street and building frontage. This area is predominantly, but not exclusively, privately owned.

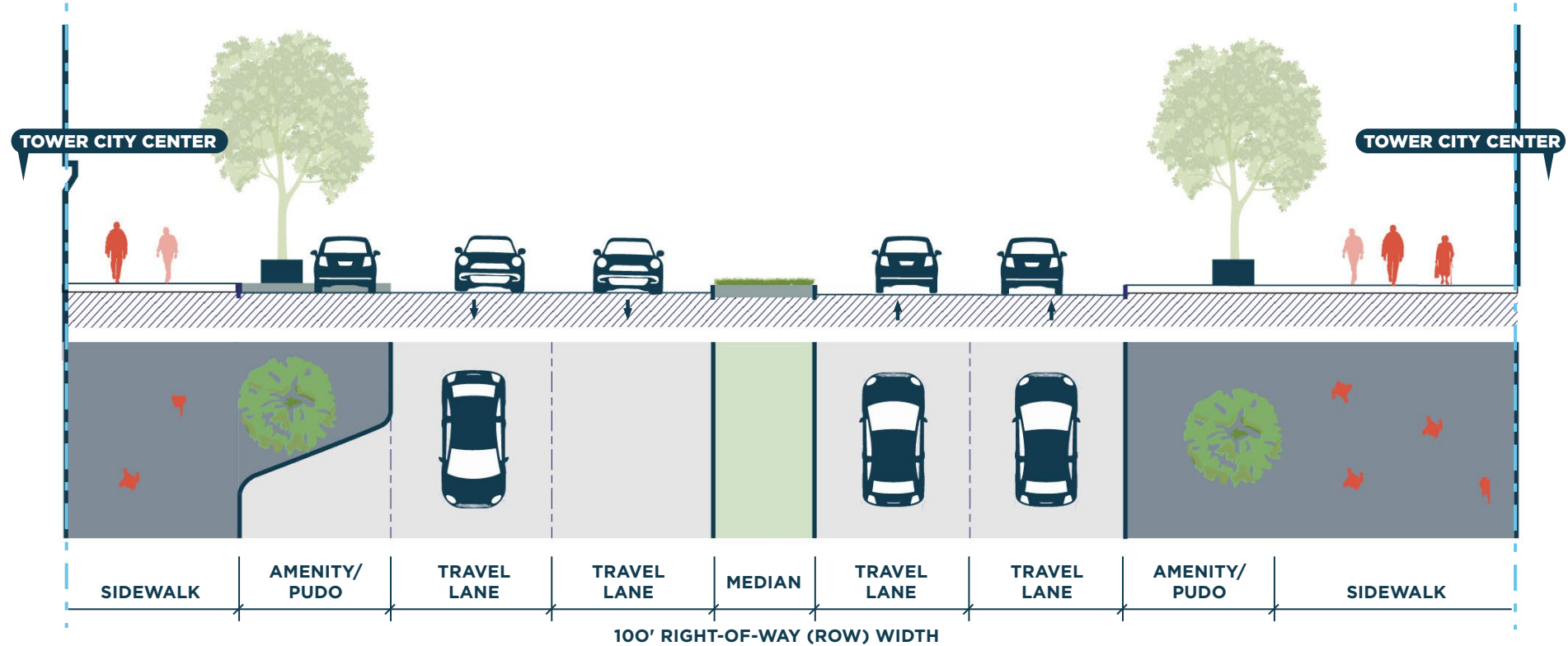
Streetscape Objectives:

- Promote street and right-of-way designs that address all features necessary to create a multimodal and dynamic public realm consistent with the city of Cleveland's Complete and Green Streets Ordinance.
- To reinforce the neighborhood's identity as walkable and accessible, prioritize pedestrian spaces, landscape design, materials, and construction quality within the right-of-way along all streets.
- Utilize a street tree canopy, water management, and other amenities along existing and proposed streets to create a distinct network of landscapes, furnishings, and character throughout the project area.
- Ensure a unified identity with consistent design and character of the streetscape and public realm while utilizing a complementary design palette within individual parks and buildings.
- Minimize utilities' physical and visual impact on corners and all areas along the streetscape and bury all utilities as required by law in the Central Business District.
- Utilize public art as a visual tool to create interest and activation and not to compensate for other design shortcomings.
- Expand pedestrian zones in areas of high GCRTA transit while also locating furnishings and amenities near these and other active pedestrian areas, including, building entries/exits. All building entrances should remain clear for emergency access.
- Provide appropriate street lighting fixtures consisting of street lights, pedestrian lights, bollards, or other decorative lighting that illuminates travel lanes and public sidewalks.



CONCEPTUAL STREETSCAPES

Prospect Avenue (Looking East)
Superior Avenue to Ontario Street



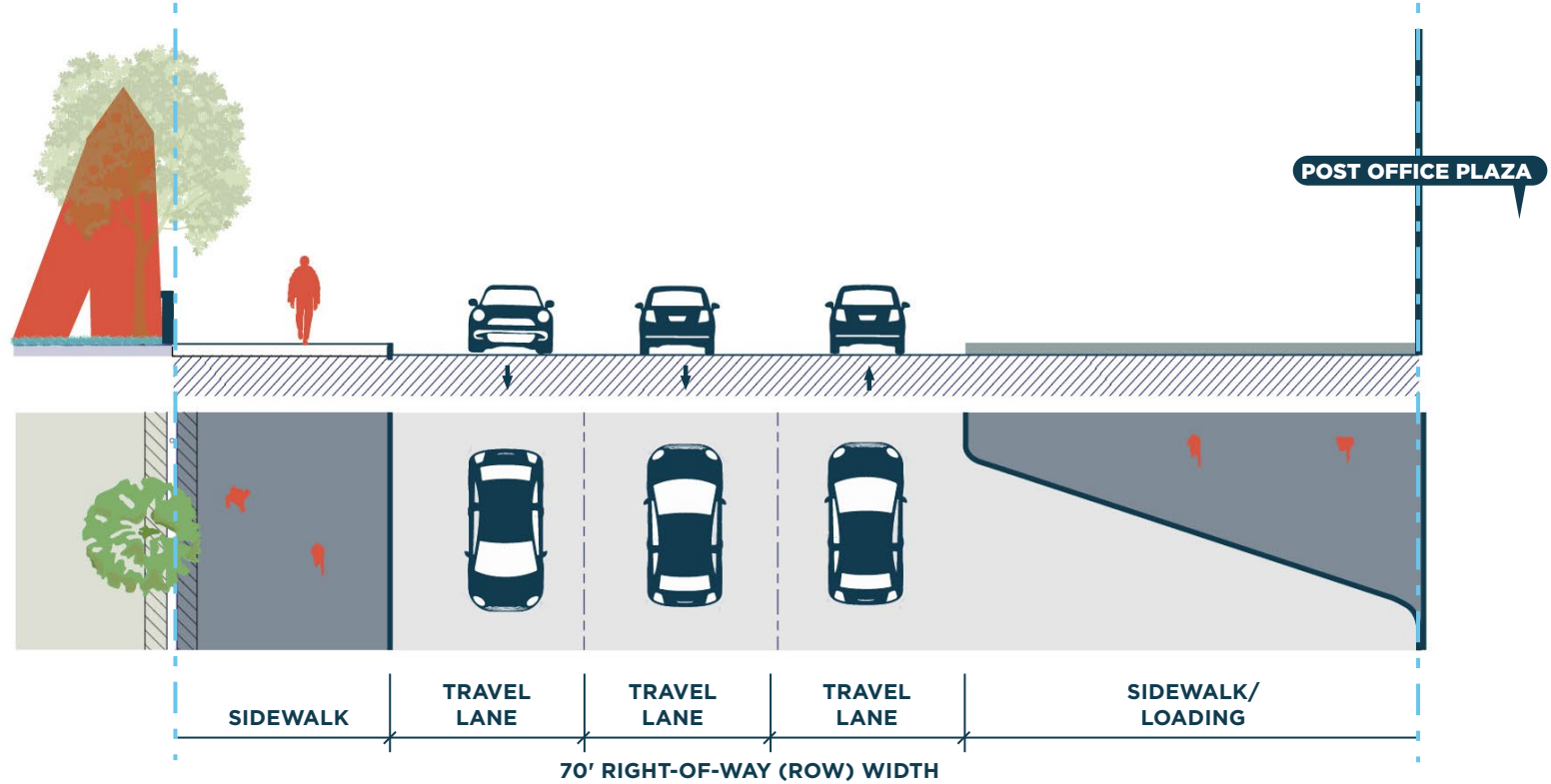
Prospect Avenue

Prospect Avenue is a major public transit corridor as people transfer between GCRTA's rail lines, all of which stop at the Tower City Center Station and multiple bus routes. Bus stops with shelters are present on both sides of Prospect Avenue to accommodate morning and afternoon ridership peaks.

Prospect Avenue Objectives:

- Expand and improve sidewalk and amenity zones where possible to provide comfortable pedestrian environments.
- Where possible, reduce crossing distances for pedestrians with amenity bump-outs and protected islands.
- Integrate GCRTA bus shelters and stops into enhanced streetscape.
- Combine amenity zones and pick-up and drop-off zones to reduce the overall width of ROW dedicated primarily to vehicle travel and parking
- Where possible, provide areas for building frontage and spill-out space.

West 6th Street (Looking North)
Huron Road to Prospect Avenue



West 6th Street

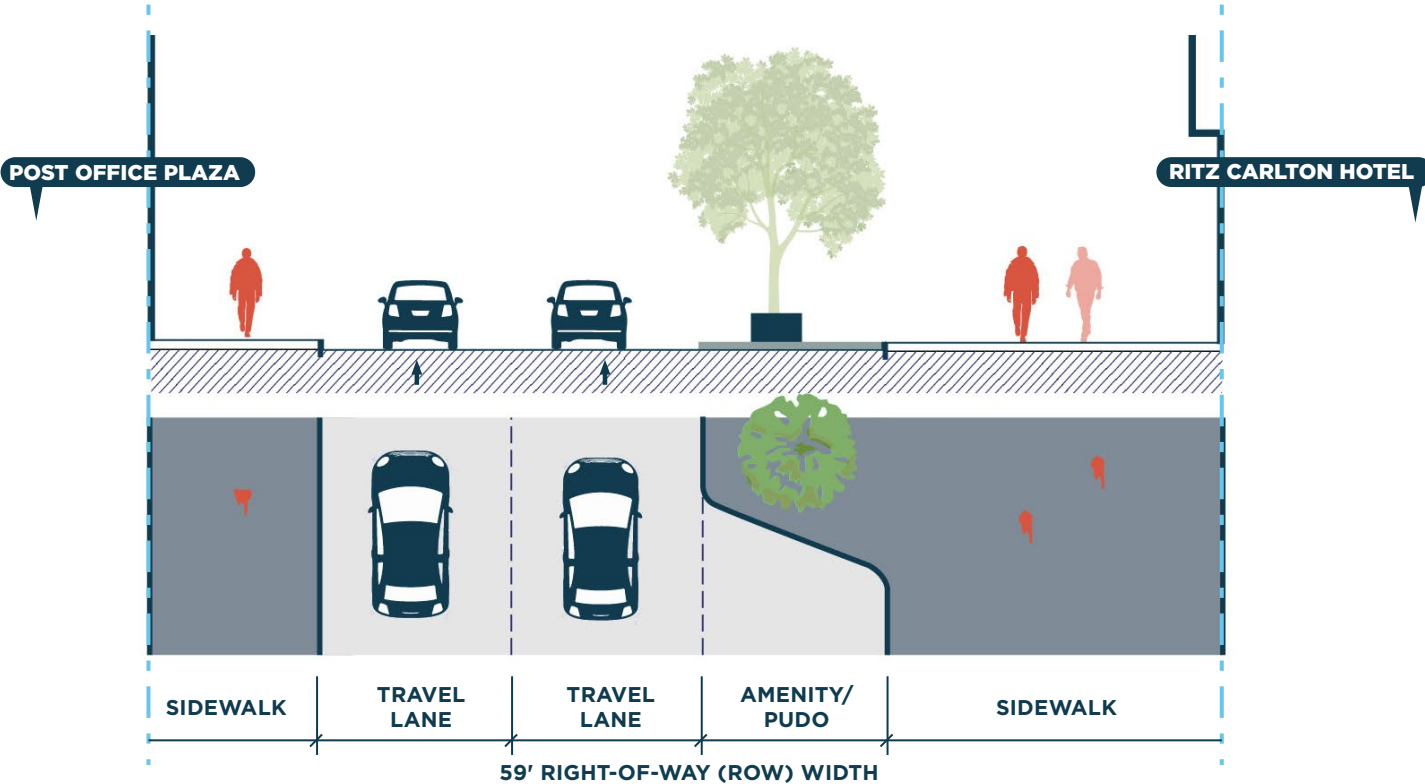
On the west side of West 6th Street, numerous bus routes serve the State Office Building and loop it to return to the east side of downtown. On the east side is loading decks and vehicular access to underground parking within the Post Office Plaza building.

West 6th Street Objectives:

- Expand and improve sidewalk and amenity zones where possible to provide comfortable pedestrian environments.
- Where possible, reduce crossing distances for pedestrians with amenity bump-outs.
- Integrate GCRTA bus shelters and stops into enhanced streetscape.

CONCEPTUAL STREETSCAPES

West 3rd Street (Looking North)
Huron Road to Prospect Avenue



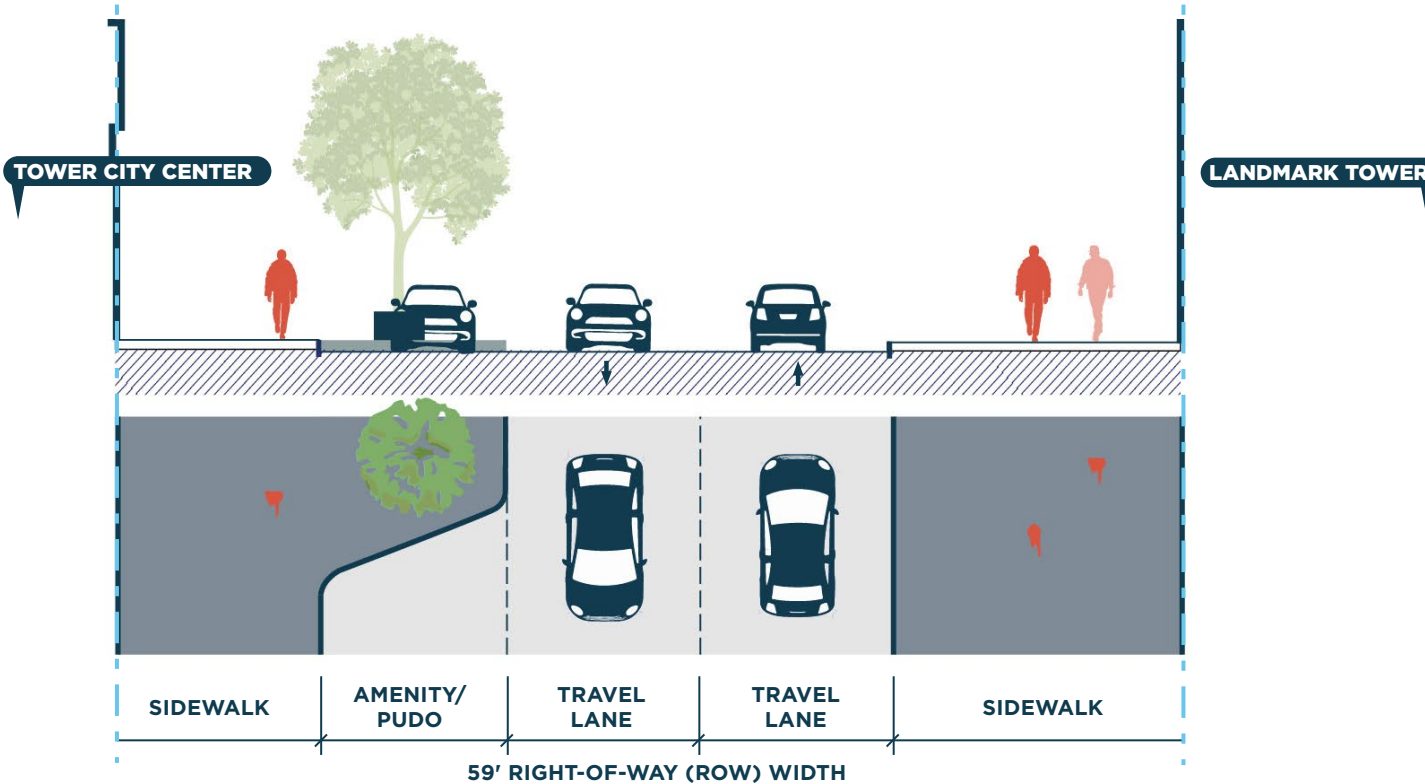
West 3rd Street

West 3rd Street is one way north bound. The main entrance to Post Office Plaza is on the west side of West 3rd street and the lobby entrance and pick up/ drop off zone for the Ritz Carlton hotel is on the east side. Pedestrian and vehicular traffic must be carefully balanced to support these uses. With the narrow row and height of the building, tree growth may be problematic.

West 3rd Street Objectives:

- Expand and improve sidewalk and amenity zones where possible to provide comfortable pedestrian environments.
- Where possible, reduce crossing distances for pedestrians with amenity bump-outs.
- Combine amenity zones and pick-up and drop-off zones to reduce the overall width of ROW dedicated primarily to vehicle travel and parking.

West 2nd Street (Looking North)
Huron Road to Prospect Avenue



West 2nd Street

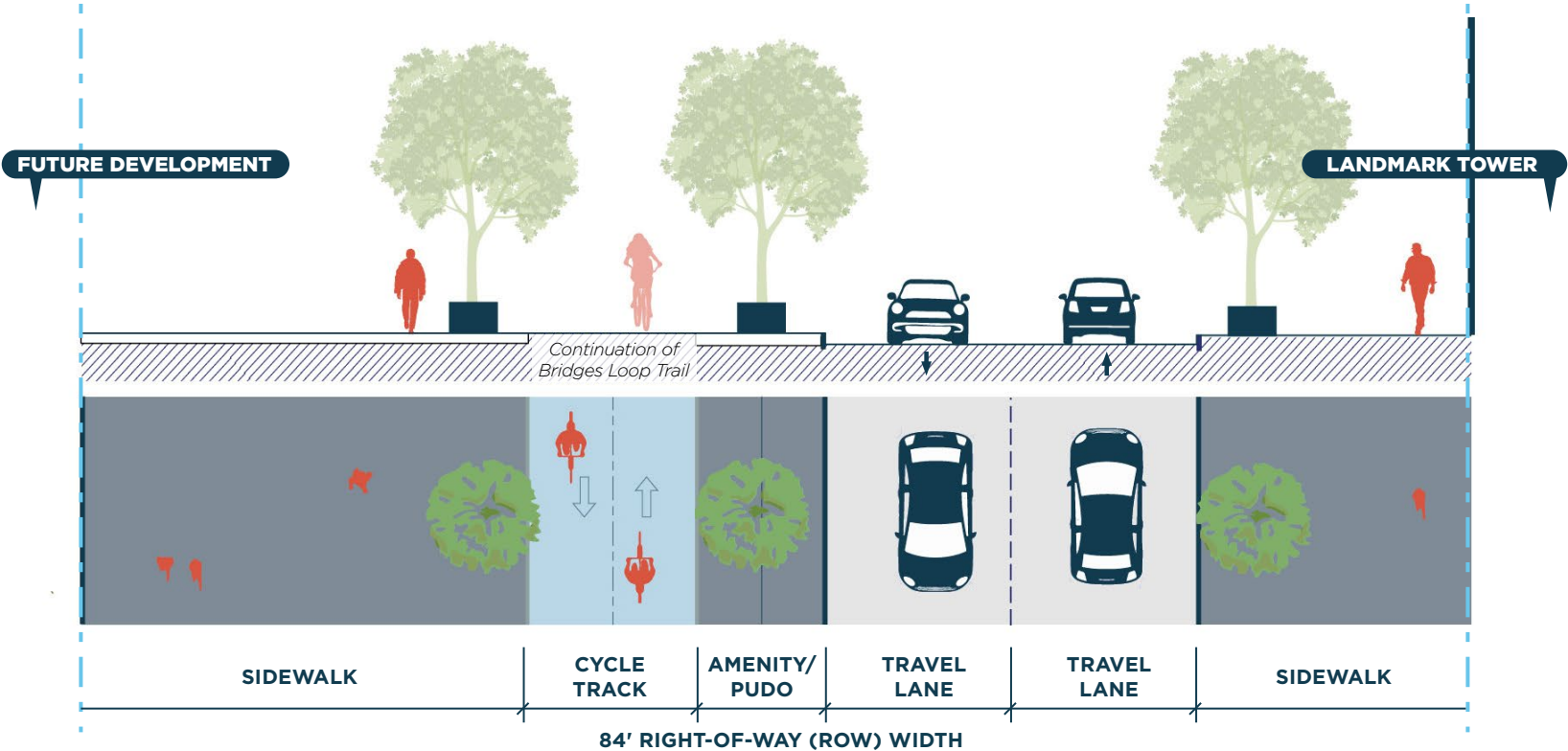
The main entrance to the Skylight Office Tower is on the west side of west 2nd street with restaurants at the corners on both the east and west sides, Pick up/ drop off zones serve both sides. With the narrow row and height of the building, tree growth may be problematic.

West 2nd Street Objectives

- Expand and improve sidewalk and amenity zones where possible to provide comfortable pedestrian environments.
- Where possible, reduce crossing distances for pedestrians with amenity bump-outs.
- Combine amenity zones and pick-up and drop-off zones to reduce the overall width of ROW dedicated primarily to vehicle travel and parking.

CONCEPTUAL STREETSCAPES

Huron Road (Looking West)
Superior Avenue to Ontario Street



Huron Road

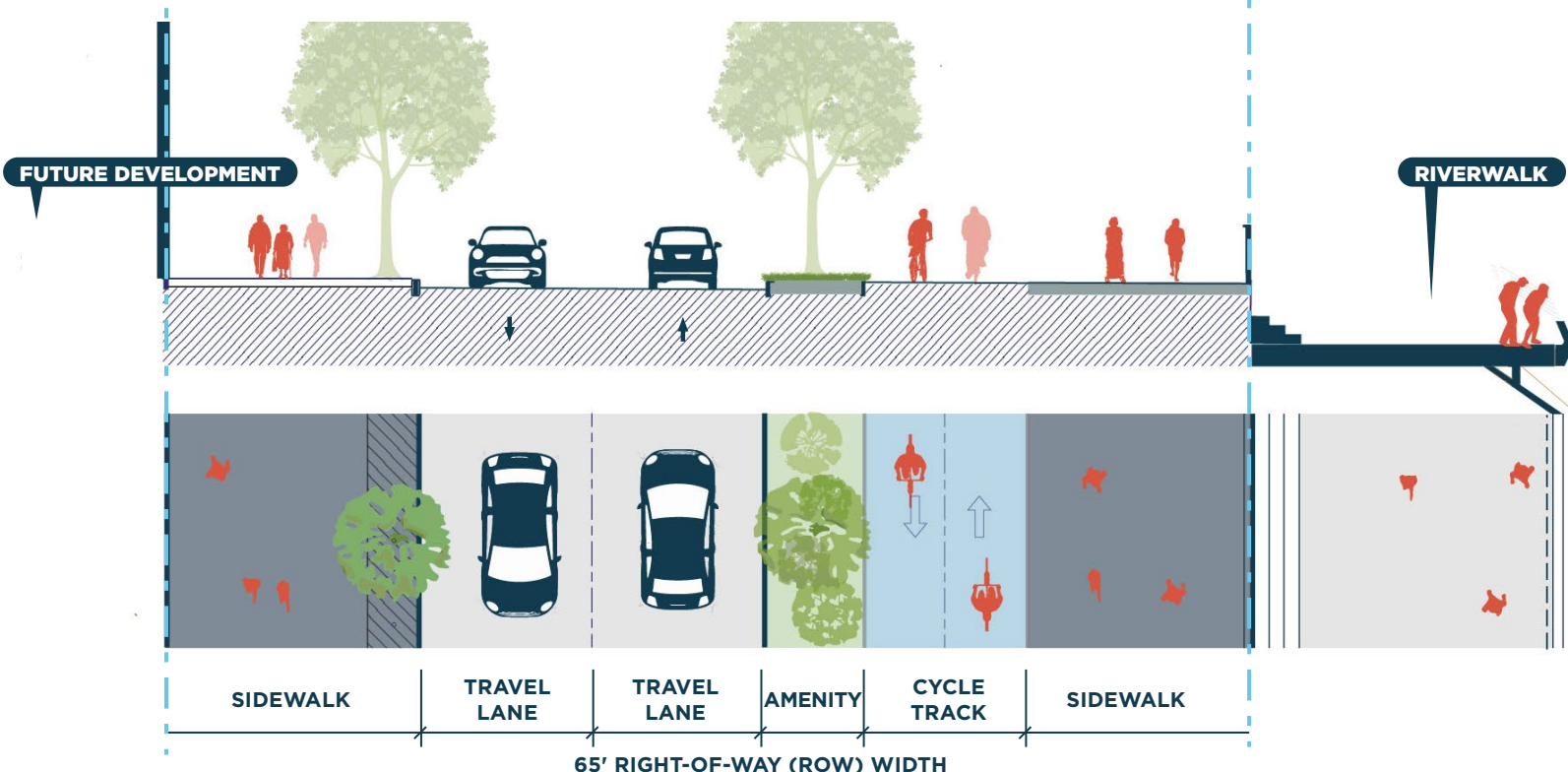
Along this segment of Huron Road, the Carl B. Stokers U.S. Courthouse is the only building on the south side while the buildings line the entire length of the right-of-way on the north.

Proposed development in the green spaces that will provide access to the riverfront or provide courtyards and outer activity for new buildings

Huron Road Objectives:

- Create special amenity zones and street crossings at courtyard steps, porch steps and city steps
- As feasible, reduce overall number of travel lanes to create a Complete Street experience.
- Expand and improve sidewalk and amenity zones where possible to provide comfortable pedestrian environments.
- Where feasible, provide street trees in a manner consistent with installation on a bridge structure.
- Coordinate proposed cycle track with ongoing designs for Memorial Bridges Loop Trail.
- Combine amenity zones and pick-up and drop-off zones to reduce the overall width of ROW dedicated primarily to vehicle travel and parking.
- Where appropriate, incorporate micro-mobility hubs at critical points along Huron Road.
- Where possible, provide area for building frontage and spill-out space.

New Roadway (Looking East)
Old River Road to Eagle Avenue

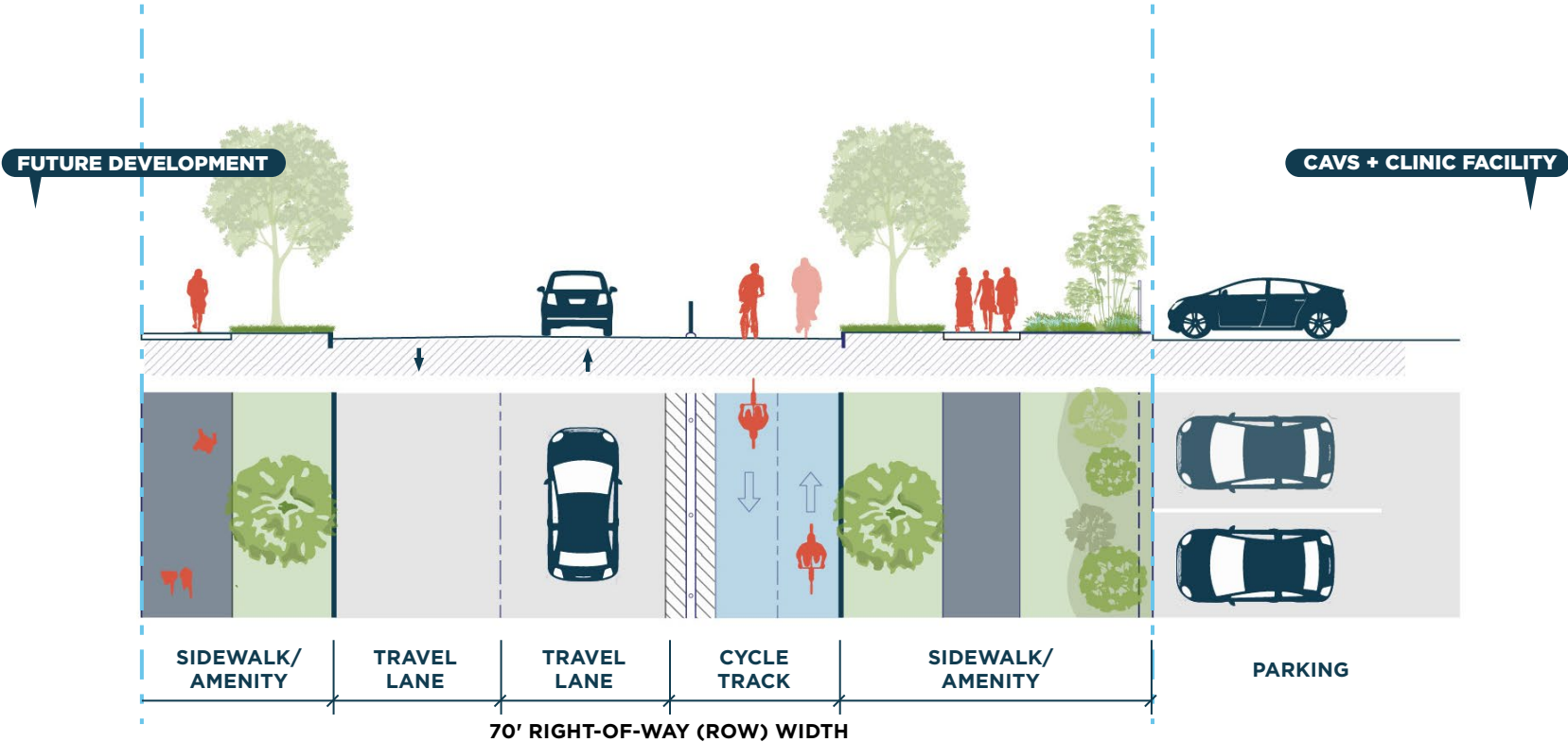


New Roadway Objectives

- New Roadway should be designed as a neighborhood boulevard and allow for balanced use of all modes of travel including pedestrian, bicycle, and micro-mobility in addition to vehicle travel.
- Sidewalk and amenity zones should be generous and provide ample space for street trees, streetlights and furnishings.
- Pedestrian crossings should be generous in width and utilize elements such as bump-outs, material changes, and/or angled face curbs to encourage ease of cross-flow.
- Combine amenity zones and pick-up and drop-off zones to reduce the overall width of ROW dedicated primarily to vehicle travel and parking.
- Where appropriate, incorporate micro-mobility hubs at critical points along New Roadway.
- Where possible, provide area for building frontage and spill-out space.

CONCEPTUAL STREETSCAPES

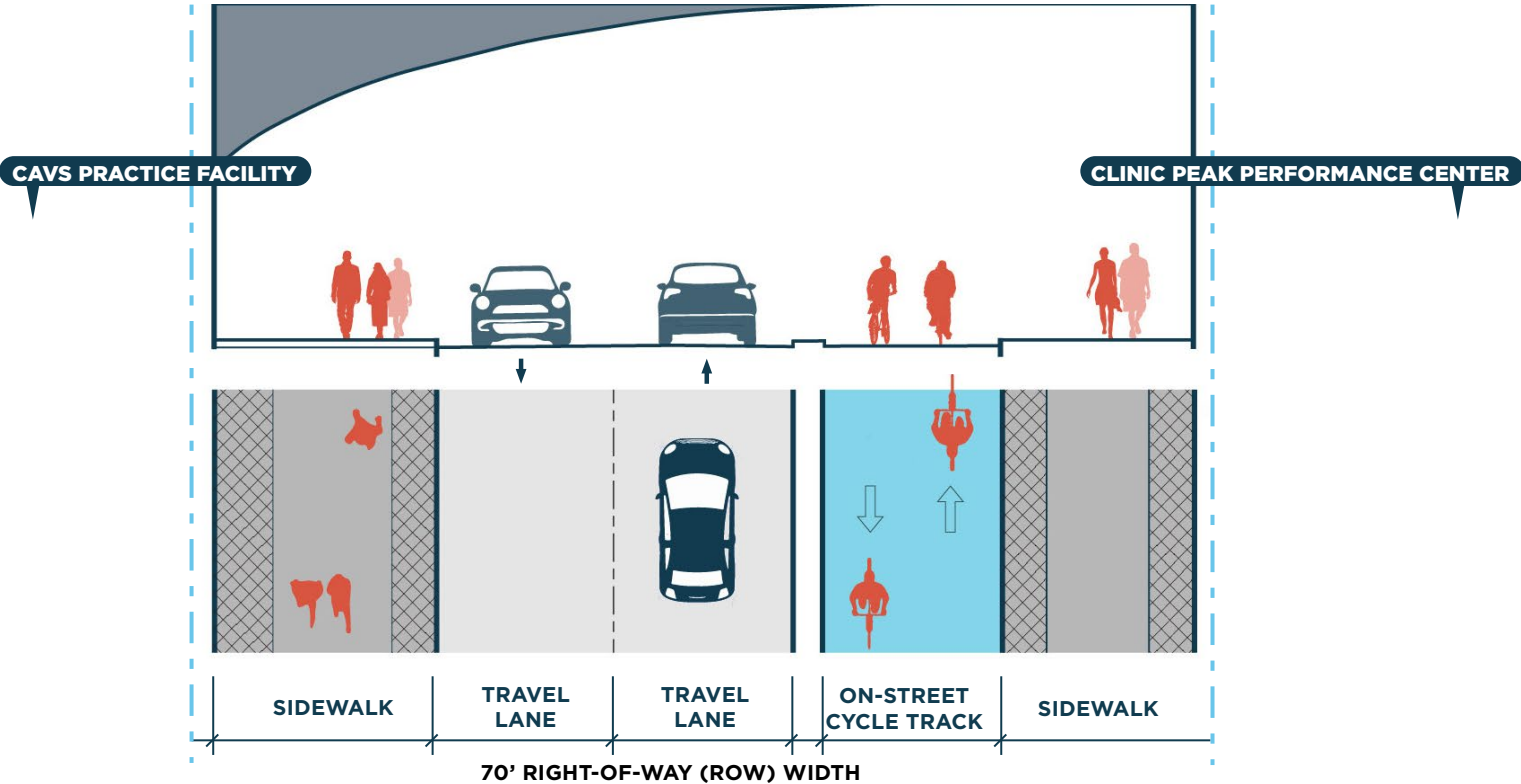
West 3rd Street (River Level - Looking North)
Huron Road to Prospect Avenue



West 3rd Street Objectives:

- West 3rd Street should be designed as a neighborhood street and allow for balanced use of all modes of travel including pedestrian, bicycle, and micro-mobility in addition to vehicle travel.
- Sidewalk and amenity zones should be generous and provide ample space for street trees, streetlights, and furnishings.
- On-street cycle tracks should be separated from travel lanes through comfortable buffer zones that consist of striped buffer zones, vertical reflectors, and/or material changes.
- Where appropriate, incorporate micro-mobility hubs at critical points along New Roadway.

Eagle Avenue Bridge (Looking East)
Huron Road to Prospect Avenue



Eagle Avenue Bridge Objectives

- To the extent feasible, Eagle Avenue Bridge should be designed as a Complete Street.
- Provide comfortable public realm environments conducive to safe passage of pedestrians, cyclists and multi-mobility users.
- Where feasible, provide planting and amenity zones in a manner consistent with bridge condition of the roadway beneath.



3

3.

ARCHITECTURAL GUIDELINES FOR PRIVATE DEVELOPMENT

OVERVIEW

Architectural Guidelines for Private Development

I. Program: Creating a Mixed Use Neighborhood

Integrate the Cuyahoga Riverfront Master Plan programming with the broader downtown Cleveland context to encourage physical and social connectivity between people with a variety of different backgrounds and experiences. Create a framework for permanent and lasting investment that strengthens the Urban Core. Topics covered include:

- Strategic Mix of Land and Building Programming
- Activating Buildings at the Street
- Locating Similar Programs
- Isolating Inactive Uses
- Invest in Diverse Housing Options

II. Form: Reinforcing Location, Scale & Character

Utilize form, scale, and density to balance multiple scales between the pedestrian experience and the larger urban fabric and to navigate the change between the riverfront and the urban center. Topics covered include:

- Legible Form and Massing
- Building Hierarchy and Scale
- Visible Interest and View Framing
- Safe and Convenient Accessible Spaces

III. Setting: Activating the Urban Form

Support Cleveland's future as a 15-minute city. Create an engaging and vibrant streetscape that promotes the pedestrian experience, establishes a distinct sense of place, facilitates multi-modal transportation, and supports the varied public and private parks and open spaces throughout the development. Topics covered include:

- Activated Building Frontages
- Articulated Entrances and Openings
- Building Approach and Accessibility
- Leverage the public realm as an integral part of the development by incorporating parks and public art throughout the Project Area

IV. Materials: Reinforcing Neighborhood Identity

Materials have different associative and historical significance that can reference a specific impression, place, and time. Materials should be selected to enhance the pedestrian experience and reinforce the identity of the neighborhood. Topics covered include:

- Material Selection and Criteria
- Visibility and Transparency
- Sustainable Materials and Practices that reduce the City's carbon footprint

V. Parking: Prioritizing Efficiency and Aesthetics

Leverage transit access and adjacency to create an interconnected multimodal transportation network and reduce demand for vehicular parking. Parking strategies should prioritize efficiency, aesthetics, and sustainability. Topics covered include:

- District Wide Approaches to Parking
- Sustainable Practices
- Structured Parking Guidelines
- Integrated Utilitarian Structures



February 7, 2025

PROGRAM

Building Uses + Locations

Creating a Mixed-Use Neighborhood

The goal of the Cuyahoga Riverfront Master Plan is to create a vibrant and comfortably scaled 15-minute neighborhood where the community lives, works, and plays. Mixing programs and amenities decreases the dependence on individual vehicles, creating more sustainable and pedestrian-friendly neighborhoods. Diverse programming creates the foundation for an 18-hour active environment, improving public safety and providing a thriving urban center for all ages, incomes, and abilities.

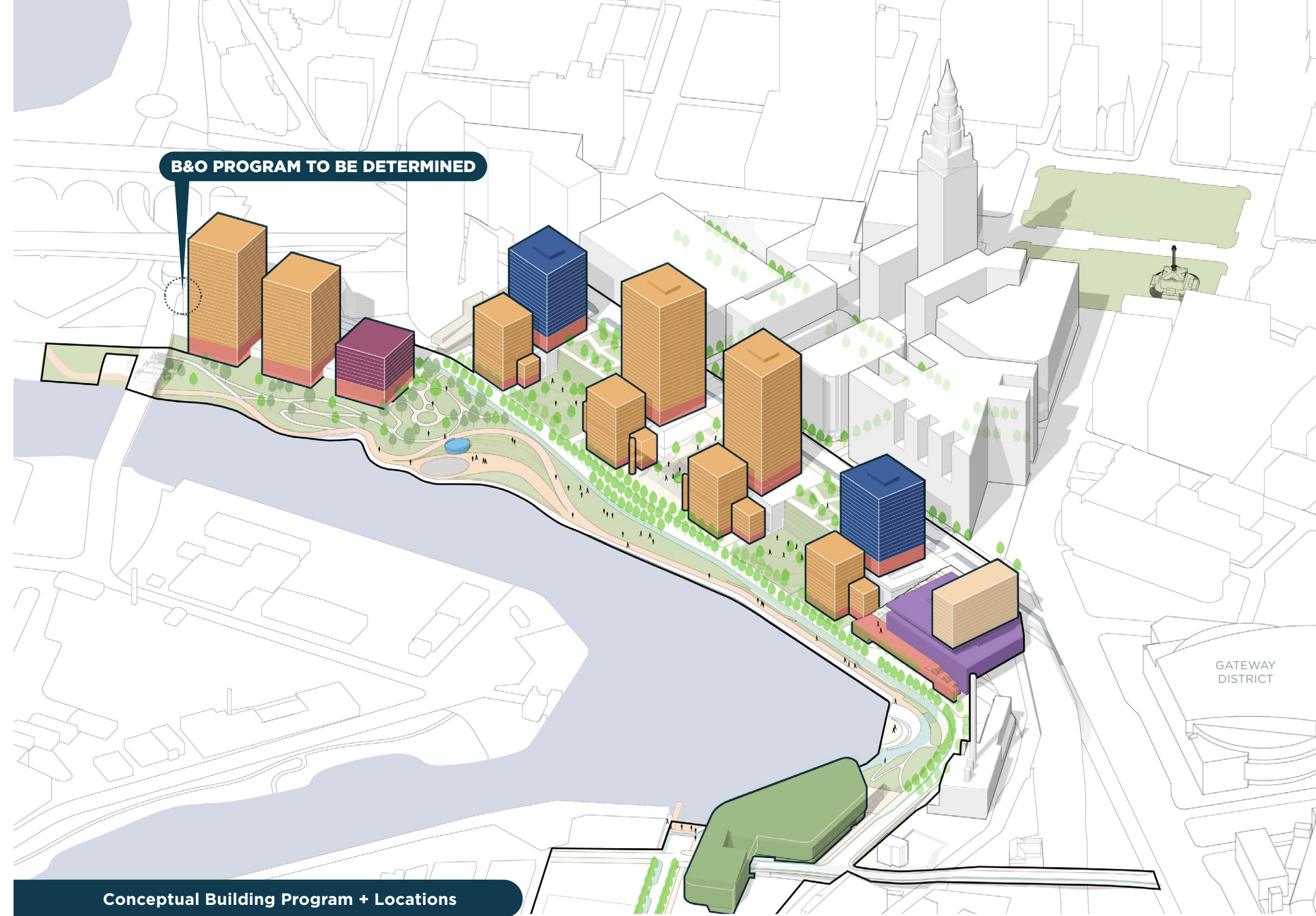
The Cuyahoga Riverfront Master Plan features a mix of residential, office, retail, and entertainment uses while integrating parks, open spaces and a variety of cultural and recreational amenities for people of all ages, abilities, and incomes. This mixed-use programming in combination with a multi-modal transportation system promotes walkability, increased social interaction, and shared public spaces. In order to increase activity along the street, more active programs, like retail and restaurants, should be located at the street level and within or adjacent to parks and open spaces.

Programs with limited activity, like parking, are encouraged to be located away from primary pedestrian passages with entrance and exit drives located to limit disruption to the pedestrian experience. Parking structures should be placed at key locations to increase shared parking between building programs with different peak hours of parking demand.

Given the timeline anticipated to complete the overall project development, market factors will likely vary from the conditions seen today. In order to provide the flexibility needed to accommodate these changing conditions, all uses demonstrated in these guidelines may be amended to allow for changing market conditions.

Program Objectives:

- Integrate the Cuyahoga Riverfront Master Development Plan with the broader downtown Cleveland context to encourage physical and social connectivity across professional disciplines, age, gender, race, ethnicity, and income.
- Strategically mix land and building program to create an enduring and vibrant neighborhood with the goal of attracting amenities that will fulfill the City of Cleveland's 15-minute vision.
- Create a locus for regionally scaled publicly accessible open space focusing on connection to the Cuyahoga Riverfront.
- Locate active uses, like retail and residential along street levels and if applicable, strategically along park spaces to enhance and street frontages and the pedestrian experience.
- Locate parking structures in key locations to maximize efficiency, reduce vehicular travel through the district, and increase shared parking between daytime and nighttime uses.
- Buildings sited adjacent to one another should locate programs similar to the adjacent structures so that similar programs face one another, whenever possible. (Like facing like)
- Programs intended for public use should be accessible from the pedestrian right-of-way.



Conceptual Building Program + Locations

- | | | | |
|---|--|---|--|
| ■ Residential | ■ Community | ■ Hotel | ■ Medical Office + Sports Performance |
| ■ Office | ■ Active Street Frontages | ■ Entertainment | ■ Cavaliers Practice Facility |

SETTING

Frontage + Activation

Activating the Urban Form

Successful streetscapes create a vibrant and walkable community by addressing design at the pedestrian scale. This includes ground floor and entry sequencing, building frontage, pedestrian passageways, program, transparency, and activation. Right-sized, permeable streetscapes create an environment conducive to pedestrian movement within the building itself and along the streetscapes around the buildings.

Neighborhoods at this scale are more inviting and approachable and establish the foundation for an active urban streetscape. Large blocks with imposing structures and fortress-like facades impede the desired activity and should be avoided. Pedestrian-oriented alleys and arcades provide safe passage, facilitate walkability, add character, and create additional opportunities for commerce and community engagement.

Given the location of the project area buildings will be visible from all angles (four-sided buildings without backs) as well as from above. Buildings should be designed to accommodate these views and each side should be developed to reinforce the pedestrian experience. The development of these multi-sided structures, in combination with the change in topography across the site creates opportunities for users to permeate the building from multiple sides and at a variety of levels. Similarly, building setbacks should address not only the public right-of-way, but also the adjacent public amenities, parks, and plazas occurring throughout the development.

Due to the large change in topography across the project area, there will be multiple levels with “ground floor” access. For clarity, these locations will be described in reference to their varied “street level elevations” above the river e.g. at West Huron Rd. (100 ft) and at New Roadway (30 ft). The objective is to activate the facades at these levels to provide a unique pedestrian experience throughout the entirety of the site and its varied entry levels. Careful consideration should be given to the grade change to ensure that the building approach and accessibility are clear and navigable across the entire site.

Setting Objectives:

- Provide a “Storefront Expansion Zone” that will enable design flexibility for storefronts and frontage for outdoor displays, dining, and seating. Recommended minimum 2’-0”.
- Avoid large blocks with significant expanses of uninterrupted facades. Recommended maximum 400’-0” between breaks.
- Vehicular, pedestrian, bicycle, and all other mobility access should be clearly identifiable and safe.
- The ceiling heights of active ground floors are typically higher than upper floors. Recommended minimum at 20’-25’.
- Entrances should be clearly defined to enhance connections between the private and public realms. Primary entrances should be prominently placed, demarcated through design, materials, signage, and lighting, and distinguished from secondary entrances.
- Building services, such as trash enclosures, recycling, ground-mounted facilities, and other such systems should be located or screened away from public view.
- Vary the style and size of articulations and openings between the street level and the upper floors of a building.
- Building entrances and openings should be located to support multi-modal transportation (bike racks, crosswalks, e-bikes, ride-share pickups, and other street amenities).
- In addition to street-facing facades, public and private outdoor spaces are to be considered frontages with building extremes and openings appropriate for ground-floor uses.



FORM

Massing + Scale

Reinforcing Location, Scale & Character

Form, scale, and density are critical elements needed to achieve the goals of the Master Development Plan (MDP). In all cases, care should be taken to break down a building's massing into forms that feel human in scale and are welcoming and comfortable.

Building footprints are represented diagrammatically and are not intended to adhere to a strict symmetrical form. Buildings will take many forms as program and context dictate. This content intends to provide guidance, not a prescriptive path.

Buildings should be designed to balance multiple scales between the pedestrian experience and the larger urban fabric. Notions of density should be balanced with issues around human scale to avoid any detrimental issues such as wind-swept plazas or solar glare.

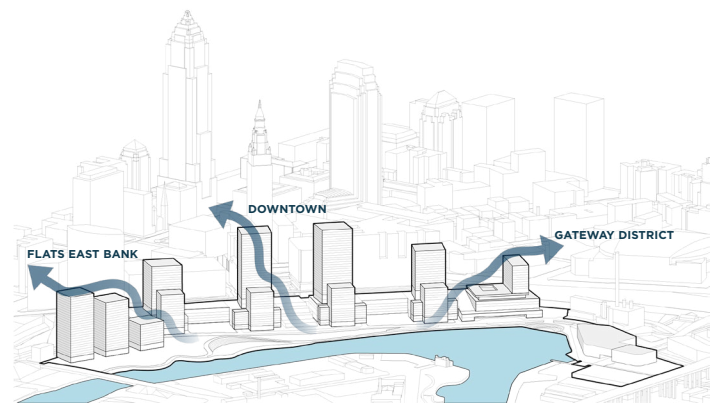
Architectural form and massing should support pedestrian movement throughout the site and down to the riverfront. Facades should be designed to create a dynamic play with shade and shadow and create legible outdoor rooms and public gathering spaces.

In order to reinforce the urban scale of downtown Cleveland, it is the goal of these guidelines that buildings be a recommended minimum height of 35 feet measured from a building's lowest street level. This minimum height is applicable to smaller scale developments along the riverfront designed to maintain view sheds and site lines. As buildings approach the urban core, they increase in scale. No maximum building height will be prescribed in these guidelines, but consideration should be given to the influential buildings that compose the city skyline and designs should be mindful of the surrounding context.

The scale of buildings should be calibrated in context to the riverfront, with heights increasing as the site shifts away from the riverfront and approaches the urban core.

Form Objectives:

- Building form and massing should be designed to create visual interest from all view corridors.
- The form and massing should be designed to create legible outdoor enclosed rooms and public spaces.
- Encourage articulation to provide shade and shadow on building facades.
- Building setbacks should address not only the public right-of-way, but also the adjacent plazas, parks, and other public amenities.
- Concentrate density on critical sites to create compact, walkable communities at each sequence of development.
- Development patterns should be designed to promote walkability, accessibility, and multi-modal transportation.
- Phases should be carefully planned to avoid fragmented development.
- Hierarchy of building height should increase as the site approaches the urban center.
- Building form should be designed to provide safe, convenient, and accessible spaces for all users and mobility types, regardless of age or ability.
- Building form and massing should preserve view angles and site lines to and from the riverfront and the existing urban core (including the Gateway District and Downtown Cleveland).
- Where appropriate, active uses or sculptural forms should be considered for roof tops.



Proposed Building Massing + Scale



PARKING

Needs + Location

Prioritizing Efficiency and Aesthetics

Parking strategies, both public and private, should prioritize efficiency, aesthetics, and sustainability. Parking quantity requirements will be determined by market demand and in accordance with the City's Transportation Demand Management (TDM) Program and will not be prescribed in these design guidelines, including vehicular, bicycle, etc. (i.e. not governed by zoning ordinance). Due to the proximity of the site to multi-modal transportation hubs and regional publicly available parking resources, parking demand is anticipated to be lower in this area of development compared to other neighboring districts.

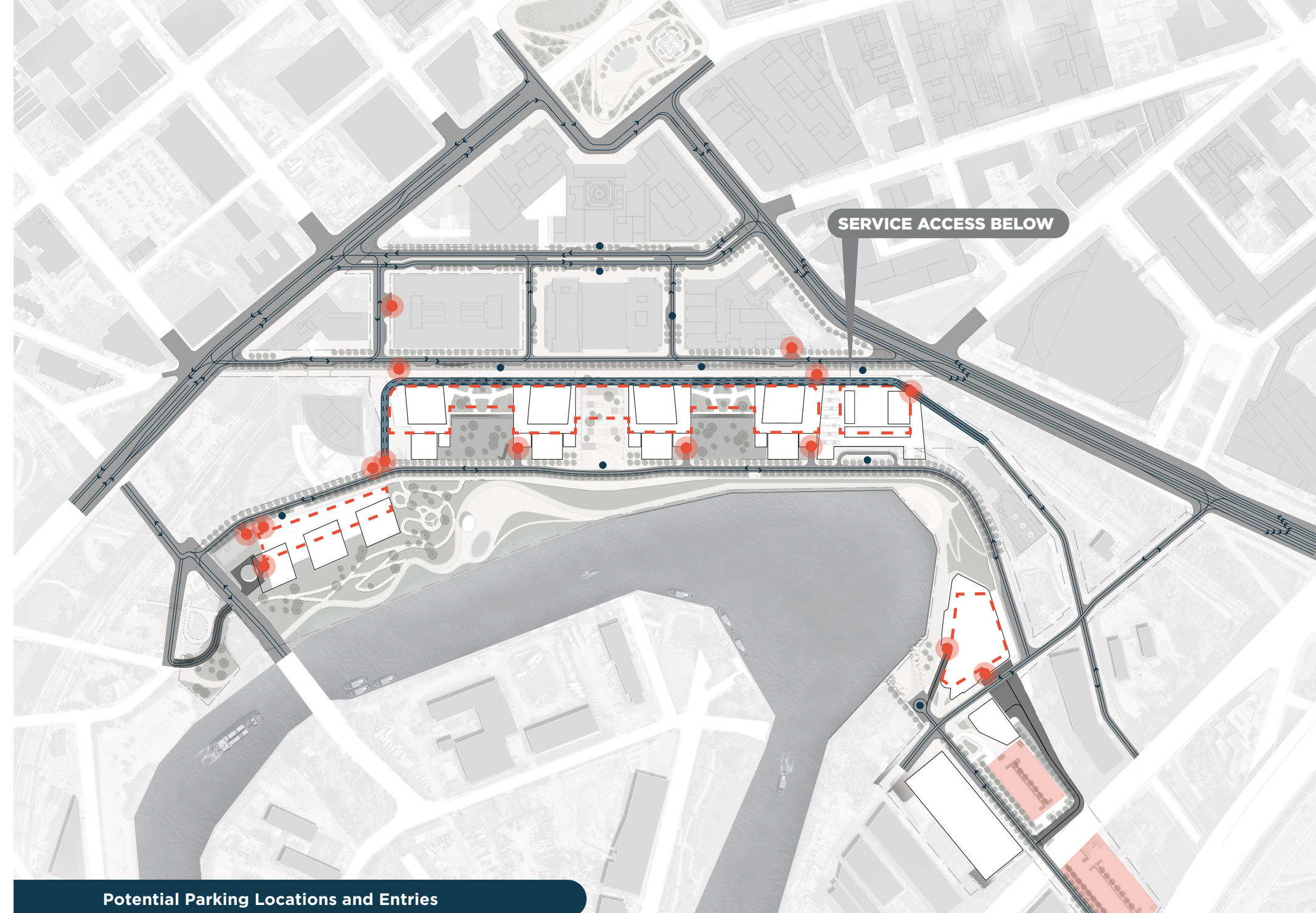
On-street parking and Pick-up / Drop-off zones may be utilized wherever feasible, necessary, and not in conflict with multi-modal curb uses and based on adjacent programs unless adjacent programs include sidewalks and bike lanes. Surface lots should be limited in size and use with pedestrian access prioritized. Surface parking edges are required to be screened from public view through landscape or other decorative elements.

Structured garages should be the primary parking solution when on-street parking alternatives are insufficient. When utilized, garages should be strategically located to maximize efficiency, reduce unnecessary traffic, and enable shared parking opportunities between daytime and nighttime uses. When garages cannot be placed away from public view, exposed structured parking should be integrated with their surroundings, whether through wrapping, screening, or context-appropriate materials. Public art, creative lighting, and innovative materials are actively encouraged to enhance the visual appeal of the garages not concealed from view.

Garages should be primarily accessed by secondary streets with limited obstruction to traffic flow on primary roads. Garages sited along the street should feature active storefront, art installations, or other engaging elements. Likewise, service elements, exit stairs, exhaust shafts, and louvers, while they are necessary utilitarian structures, may be incorporated into building architecture or standalone as important elements of the streetscape and opportunities for celebration, public art and to enliven the pedestrian experience. Refer to the Transportation + Mobility Section in Guidelines for Public Infrastructure chapter for additional information on vehicular circulation, garage ingress/egress, and service access.

Parking Objectives:

- A district-wide approach should be taken to decouple parking strategies, create a distributed network of parking, and promote the Transportation Demand Management (TDM) Program Standards which will increase the use of multi-modal transportation and alleviate overall parking demand.
- Parking, stacking, and vehicle circulation are not appropriate between a principal building and the right-of-way.
- Parking requirements will be determined by market demand and will not be prescribed in these design guidelines. More information on parking and other mobility strategies can be found in the Mobility Plan for Cuyahoga Riverfront.
- Off-street parking should be structured when possible.
- Garages should be wrapped, screened, or built using materials complementary to the surrounding context. All screening should be designed to accommodate clear view triangles in association with corner locations and vehicle access points.
- Parking design should include measures to mitigate negative off-site impacts, such as noise, high traffic levels, and light spillage.
- Use sustainable technologies to manage stormwater runoff in surface parking lots.



Potential Parking Locations and Entries

- Garage Ingress/Egress
- Potential Parking Garage Footprints
- Roadways + Service Drives
- Surface Parking

MATERIALS

Quality + Character

Reinforcing Neighborhood Identity

Material selection should provide a diverse and distinct architectural character, which remains within a range related to adjacent contexts and Cleveland’s historic landmarks.

Materials have different associative and historical significance that can reference a specific impression, place, and time. Material selection should invite the user into a welcoming environment that reflects its presence in downtown Cleveland but establishes a distinct character within the Cuyahoga Riverfront Master Plan.

Material texture also has associative features. For example, smooth finishes, like glass and metal, can evoke a more contemporary aesthetic. Innovative façade materials can provide both functional and aesthetic benefits while supporting the building’s sustainability objectives.

The material palette should be contemporary, permanent, tactile, and in colors complimentary to the surrounding context. The architectural vocabulary includes, but is not limited to: stone, masonry, glazing, terra cotta, precast concrete, metal panel, and mass timber.

Architectural details, materiality, and scale are essential to enhancing the neighborhood’s character. Building materials should be detailed and articulated to add variation and texture. Entrances should be open, transparent, and welcoming, navigating the space between the private and public realms.

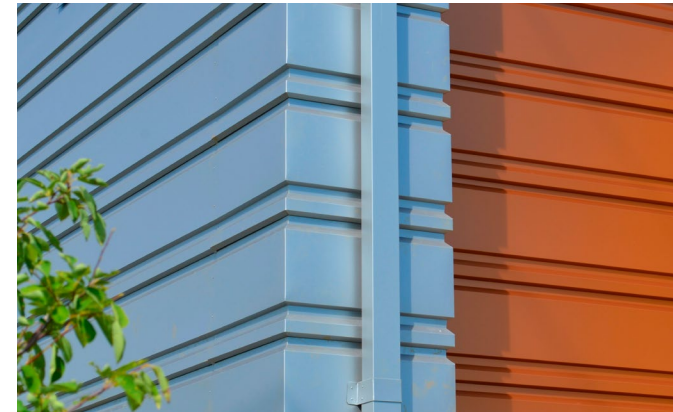
The material palette is designed to be open-ended, enabling it to evolve to accommodate advancements in technology and manufacturing. These guidelines are intended to provide a general overview of the potential materials and material uses available and do not prescribe the use of specific architectural styles.

Materials Objectives:

- Buildings should be designed for permanence. Materials should be selected to maximize lifespan and be designed for easy replacement.
- Buildings should be constructed of high-quality and durable materials that reflect the vision of the Cuyahoga Riverfront Master Development Plan.
- Designs should include transparency (especially at the street level), articulation, shading, form, materials, and other elements that link indoor and outdoor spaces and create a vibrant urban character.
- The size and pattern of materials should be sensitive to the surrounding context within downtown Cleveland, complementing the notable historic structures, such as Terminal Tower and the B&O Station.
- The material palette is intended to be open-ended, allowing it to evolve as the functions and size of the community develop over time and to accommodate changes in technology and manufacturing.
- Material selection and architectural design features should reflect the City’s sustainability objectives help to reduce it’s carbon footprint. Whenever possible, projects should use locally sourced materials and materials derived from renewable resources. Materials should be selected to increase building energy efficiency and reduce carbon footprint.
- Special design provisions must be implemented to protect migratory birds and other wildlife.



MASONRY



METAL



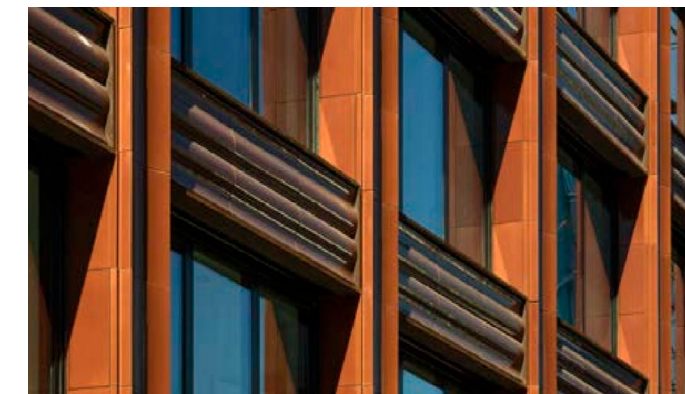
GLASS



PRECAST CONCRETE



MASS TIMBER



TERRA COTTA

ARCHITECTURAL LIGHTING

Quality + Character

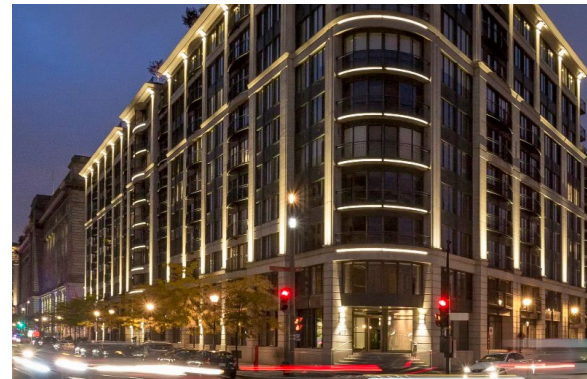
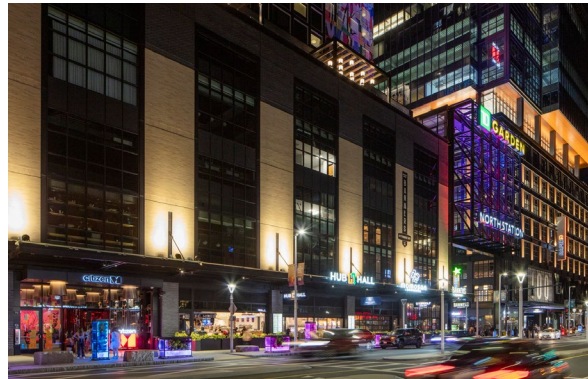
Pursuing Creativity and Longevity

Lighting adds to both the character and safety of public streets, as well as contributing to the overall success of a neighborhood. Night lighting will help prolong street life, after business hours, and increase pedestrian safety.

Lighting should be designed to complement and enhance architectural features while preventing unwanted glare, light trespass, uplighting, and over-illumination. Special provisions must be implemented to protect migratory birds and other wild life.

Nighttime illumination of building and/or tenant signage should be mindful of adjacent color temperature, location, and its impact on the public realm.

Creative lighting solutions are encouraged especially at major exterior facades.



Design Review - Lighting & Signage

- All lighting and signage submittals will be reviewed in conjunction with the building and/or tenant design and with respect to the overall vision of the Master Development Plan. Signage and lighting submittals will be reviewed by the Cleveland Planning Commission and all other reviewing bodies included in the Design Review Process.

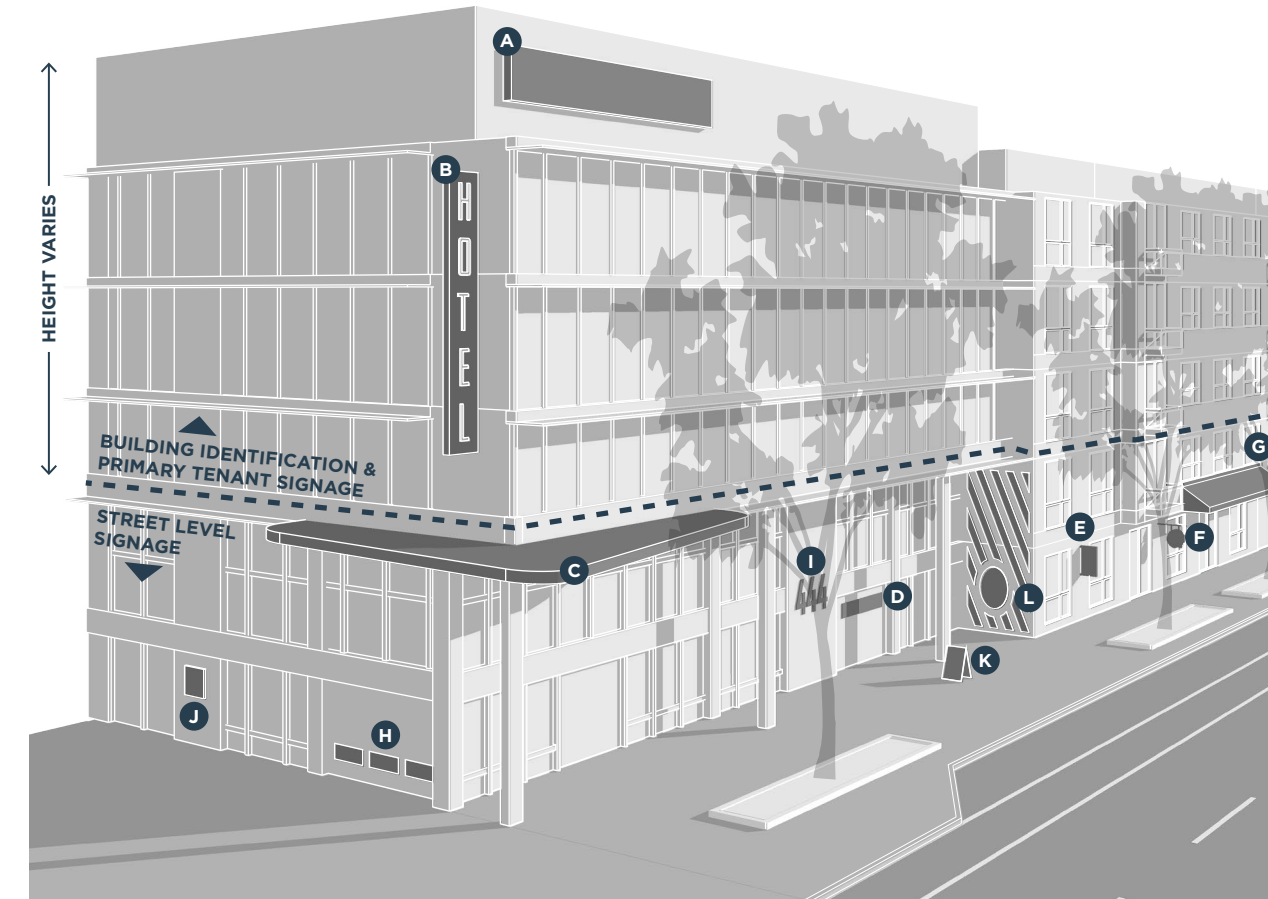
BUILDING & TENANT SIGNAGE

Quality + Character

Conveying Character and Identity

It is essential for signage to represent the character of the development. Signage conveys the identity of the neighborhood, links key destinations, and promotes the safety and well-being of the community. All signage in the development should be mindful of the Cuyahoga Riverfront Master Plan's comprehensive design strategy.

Future signage plans for each sequence will need to be developed to establish detailed design and location requirements.



Signage Objectives:

- Identify gateways and key entry points.
- Convey clear wayfinding to parking and building facilities and for service, Pick-up / Drop-off (PUDO) drives.
- Convey clear and appropriate means for identifying buildings and their tenants, and where possible aim to minimize the number, type, and size of signage to convey the uses within a building.
- Provide maximum and minimum allowable signage areas.
- Enhance the pedestrian experience through a variety of tenant storefront signage, which should be integrated into the respective storefront design.
- Establish guidelines for building identification signage or primary upper-level tenant signage.
- Develop criteria for large-format advertisements.
- Allow for proper integration and regulation of technology (i.e. touch screens, digital signs, video boards, and marquees).
- Improve the visual aesthetics, reduce visual clutter and the overuse of signage.
- As buildings are constructed and in accordance with zoning regulations explore the development of a master signage package for the district to ensure consistency.



4. RESOURCES

RESOURCES

Supporting Documentation

Resources

- The Cuyahoga Riverfront Master Plan, Adjaye Associates, May 19, 2023
- Circular Cleveland Roadmap | [Cleveland_Report_Roadmap](#)
- City of Cleveland Transportation Demand Management (TDM) Program | [TDM_Program_Standards](#)
- City of Cleveland Complete and Green Streets Ordinance | [Complete & Green Streets | City of Cleveland Ohio](#)
- 2030 City of Cleveland Climate Action Plan | [CAP Draft](#)
- Cleveland City Planning Commission Design Review Applicant Guide | [Applicants Guide](#)
- Cleveland Flats Design Guidelines, City of Cleveland, Ohio, March 2022
- Cleveland Moves: Cleveland's 5-year Multimodal Transportation Plan | [Cleveland Moves](#)
- Mobility Plan for the Cuyahoga Riverfront Master Plan, Bedrock,
- Reimagining Downtown Cleveland, Downtown Cleveland Inc., | [2023 Reimagining-Downtown-Plan](#)
- Vision for the Valley, approved by Cleveland City Planning Commission July 16, 2021

